

Indiana Department of Transportation

County Clark County

Route CR 403/Stacy Road

Des. No. 1802805

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	County Road 403 at Stacy Road, Clark County, Indiana
Designation Number:	1802805
Project Description/Termini:	The project involves intersection improvements, including construction of a single-lane roundabout, at the intersection of County Road (CR) 403 and Stacy Road in Clark County, IN. The logical termini for this project will be approximately 575.0 feet west, 455.0 feet east, 200.0 feet north, and 355.0 feet south of the existing intersection.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

_____	_____	_____	_____
ESM Signature	Date	ES Signature	Date
_____		_____	
FHWA Signature		Date	

Release for Public Involvement

_____	_____	_____	_____
ESM Initials	Date	ES Initials	Date

Certification of Public Involvement _____
 Office of Public Involvement Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.
 Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Aaron M. Toombs / United Consulting

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. The level of public involvement should be commensurate with the proposed action.

Does the project have a historic bridge processed under the Historic Bridges PA*? Yes No
If No, then: Opportunity for a Public Hearing Required? X

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks: Notice of Entry letters were mailed to potentially affected property owners near the project area on November 8, 2019... To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Historic Properties Affected" was published in the Clark County News and Tribune on February 10, 2021... The project will meet the minimum requirements described in the current Indiana Department of Transportation (INDOT) Public Involvement Manual...

Public Controversy on Environmental Grounds Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes No X

Remarks: At this time, there is no substantial public controversy concerning impacts to the community or to natural resources.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Clark County Highway Department INDOT District: Seymour
Local Name of the Facility: County Road 403 and Stacy Road

Funding Source (mark all that apply): Federal X State Local X Other*

*If other is selected, please identify the funding source:

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PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

The need for this project is derived from the high accident rate at the intersection, as evidenced from a Road Hazard Analysis Tool (RoadHAT) analysis completed for the intersection by United Consulting. Crash data from a three year period spanning January 2014 to December 2016 found that 27 accidents had occurred at the intersection, which was 5.19 standard deviations higher than the expected number of crashes at similar intersections.

The purpose of the project is to reduce the frequency and severity of accidents at the intersection of CR 403 and Stacy Road.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Clark Municipality: Charlestown

Limits of Proposed Work: The project extends 200.0 feet north of the existing intersection, 455.0 feet east along CR 403, 575.0 feet west along CR 403, and 355.0 feet south east along Stacy Road.

Total Work Length: 0.19 Mile(s) Total Work Area: 4.1 Acre(s)

Yes¹ **No**

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?
If yes, when did the FHWA grant a conditional approval for this project?

<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input style="width: 80%;" type="text"/>	

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The project is located at the intersection of CR 403 and Stacy Road in Clark County, approximately 2.3 miles Northeast of Sellersburg and 3.25 miles southwest of Charlestown.

CR 403 is functionally classified as a Principal Arterial – Other roadway within the project area. CR 403 is comprised of two 12.0 foot travel lanes, with one heading southwest and one heading northeast. Stacy Road is functionally classified as a Local roadway within the project area. Stacy Road is comprised of two 10.0 foot travel lanes, with one heading southeast and one heading northwest. The existing intersection between CR 403 and Stacy Road is a “T” intersection with CR 403 having free flow traffic and Stacy Road stopped via signage. No pedestrian facilities are located within the project limits. Stormwater runoff is channeled to roadside ditches via sheet flow. Two small culverts are located within the project area and facilitate stormwater conveyance. Posted speed limits are 50 mile per hour (mph) for CR 403 and 30 mph for Stacy Road. Surrounding features include agricultural fields, residential properties, one religious facility, and a limestone quarry.

The preferred alternative will involve construction of a single lane roundabout at the existing intersection. The center of the roundabout will be shifted to the north to avoid the church parcel and utility conflicts. The inscribed diameter will be 130.0 feet and include an 18.0 foot circulatory lane width with a 13.0 foot truck apron. The intersection improvement will slow vehicles on CR 403 and provide turning movements to and from Stacy Road. Existing culverts within the project area will be replaced with 36.0 inch diameter corrugated metal pipes (CMP) with riprap placed at the outlets for scour protection.

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During construction, Maintenance of Traffic (MOT) will involve a full road closure and use of a detour route. Please see the MOT section of this CE document for further details regarding MOT and the proposed detour.

The preferred alternative will meet the purpose and need of the project by providing a single lane roundabout which will slow the vehicles on CR 403, provide safer turning movements onto and off Stacy Road, and as a result, will reduce the frequency and severity of accidents at the intersection.

The logical termini for this project extend 200.0 feet north of the existing intersection, 455.0 feet east along CR 403, 575.0 feet west along CR 403, and 355.0 feet south east along Stacy Road and were logically chosen to minimize the impacts of the project. The project is considered to provide independent utility as the completion of the project will not dictate the outcome of any other projects in the surrounding area. Construction of this project could commence without impacting, affecting, or influencing any neighboring projects.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

No Build Alternative:

The only alternative evaluated for this project was the 'No Build' option. This alternative proposed utilization of the existing facilities without modifications. The selection of this alternative will not meet any of the objectives established by the purpose and need statement. As a result, this alternative was discarded from further consideration.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe)

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

ROADWAY CHARACTER: CR 403

Functional Classification:	<u>Minor Arterial</u>				
Current ADT:	<u>12,943</u>	VPD (2020)	Design Year ADT:	<u>19,614</u>	VPD (2042)
Design Hour Volume (DHV):	<u>984</u>	Truck Percentage (%)		<u>5</u>	
Designed Speed (mph):	<u>50</u>	Legal Speed (mph):		<u>50</u>	

Existing	Proposed
-----------------	-----------------

Number of Lanes:	2		2	
Type of Lanes:	Travel Lanes		Travel Lanes	
Pavement Width:	24.0	ft.	24.0	ft.
Shoulder Width:	2.0	ft.	2.58	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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ROADWAY CHARACTER: Stacy Road

Functional Classification: Local Road
 Current ADT: 1,777 VPD (2020) Design Year ADT: 2,693 VPD (2042)
 Design Hour Volume (DHV): 147 Truck Percentage (%): 5
 Designed Speed (mph): 30 Legal Speed (mph): 30

Existing Proposed

Number of Lanes:	2		2	
Type of Lanes:	Travel Lanes		Travel Lanes	
Pavement Width:	20.0	ft.	22.5	ft.
Shoulder Width:	N/A	ft.	N/A	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

DESIGN CRITERIA FOR BRIDGES:

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
(Rating, Source of Information)

Existing Proposed

Bridge Type:	N/A		N/A	
Number of Spans:	N/A		N/A	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	N/A	ft.	N/A	ft.
Outside to Outside Width:	N/A	ft.	N/A	ft.
Shoulder Width:	N/A	ft.	N/A	ft.
Length of Channel Work:			N/A	ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks:

Two small structures are located within the project area.

Structure #1 crosses under Stacy Road approximately 50.0 feet south of the centerline of CR 403. The existing structure is a 15.0 inch diameter concrete pipe with a 2.6 acre drainage area. The existing structure will be replaced with a 36.0 inch diameter pipe. Class 1 riprap will be placed at the downstream end of the new structure for scour protection.

Structure #2 crosses under CR 403 approximately 220.0 feet west of the centerline of Stacy Road. The existing structure is a 12.0 inch concrete pipe with a 4.9 acre drainage area. The existing structure will be replaced with a 36.0 inch diameter pipe. Class 1 riprap will be placed at the downstream end of the new structure for scour protection.

Will the structure be rehabilitated or replaced as part of the project? **Yes** **No** **N/A**

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The MOT for this project will require a full road closure and use of a detour route. The detour route will include John Noble Road, High Jackson Road, and Stoney Point Road/Bethany Road. The proposed detour route will have an added travel length of approximately 3.92 miles. Pedestrian features are not present at the project location and as a result, no pedestrian MOT will be required for this project.

The closures will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated and all inconveniences will cease upon project completion. Delays may occur during construction but will cease with project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 315,000 (2020) Right-of-Way: \$ 91,500 (2020) Construction: \$ 2,257,500 (2021-2022)

Anticipated Start Date of Construction: April 2022

Date project incorporated into STIP July 2, 2019

Is the project in an MPO Area? **Yes** **No**

If yes,

Name of MPO Kentuckiana Regional Planning and Development Agency (KIPDA)

Location of Project in TIP Page 112

Date of incorporation by reference into the STIP June 20, 2019

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.22	0.00
Commercial	0.00	0.00
Agricultural	0.62	0.00
Forest	0.28	0.00

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Wetlands	0.00	0.00
Other:	0.00	0.00
Other:	0.00	0.00
TOTAL	1.12	0.00

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and their impacts on the environmental analysis should be discussed.

Remarks:

The existing typical right-of-way (ROW) widths within the project corridor extend approximately 55.0 feet from each side of the centerline of the existing roadways for both CR 403 and Stacy Road. The proposed ROW widths within the project corridor will extend approximately 80.0 feet north from the centerline of CR 403, and will remain unchanged along Stacy Road. The adjacent land use is predominately residential with some agricultural fields, sparsely forested land, a religious facility, and several industrial properties located in the surrounding area.

The project requires approximately 1.12 acres of permanent ROW acquisition from neighboring residential and agricultural properties, including ROW from one religious facility. The project does not require any temporary ROW acquisition, advanced acquisition, or reacquisition.

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Jurisdictional Ditches			
Federal Wild and Scenic Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Natural, Scenic or Recreational Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Nationwide Rivers Inventory (NRI) listed	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Outstanding Rivers List for Indiana	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Navigable Waterways	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Based on a desktop review, a site visit on May 7, 2020 by United Consulting, the aerial map of the project area (Appendix B, B-2), and the water resource map in the Red Flag Investigation (RFI) report (Appendix E, E-9) there are six streams, rivers, watercourse or jurisdictional ditches within the 0.5 mile search radius. No streams, rivers, watercourses, or jurisdictional ditches are present within the project area. Therefore, no impacts are expected.

Early coordination letters were sent to Indiana Department of Natural Resources Division of Fish and Wildlife (IDNR DFW), United States Fish and Wildlife Service (USFWS), and United State Army Corps of Engineers (USACE) on April 20, 2020. The USACE did not respond to the early coordination letter. The IDNR DFW responded on May 20, 2020 with several recommendations to avoid or minimize impacts to fish, wildlife and botanical resources (Appendix C, C-3 to C-4). The USFWS responded with an email on April 28, 2020 (Appendix C, C-18 to C-19), indicating that the project is within range of the Indiana bat and

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Northern long-eared bat (NLEB) and should follow the programmatic consultation process in addition to the project being located within the karst area of Indiana and that their list of standard recommendations will apply to the project. All applicable IDNR DFW recommendations are included in the Environmental Commitments section of this CE document.

Other Surface Waters	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: Based on a desktop review, a site visit on May 7, 2020 by United Consulting, the aerial map of the project area (Appendix B, B-2), the water resource map in the RFI report (Appendix E, E-9) there are three other surface waters within the 0.5 mile search radius. No other surface waters are present within the project area. Therefore, no impacts are expected.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on May 20, 2020 with several recommendations to avoid or minimize impacts to fish, wildlife and botanical resources (Appendix C, C-3 to C-4). The USFWS responded with an email on April 28, 2020 (Appendix C, C-18 to C-19), indicating that the project is within range of the Indiana bat and Northern long-eared bat (NLEB) and should follow the programmatic consultation process in addition to the project being located within the karst area of Indiana and that their list of standard recommendations will apply to the project. All applicable IDNR DFW recommendations are included in the Environmental Commitments section of this CE document.

Wetlands	Presence	Impacts	
		Yes	No
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.00 acre(s) Total wetland area impacted: 0.00 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
N/A	N/A	N/A	N/A	N/A

Wetlands (Mark all that apply)	Documentation	ES Approval Dates
Wetland Determination	<input type="checkbox"/>	<input type="checkbox"/>
Wetland Delineation	<input type="checkbox"/>	<input type="checkbox"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="checkbox"/>
Mitigation Plan	<input type="checkbox"/>	<input type="checkbox"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>
Substantially increased project costs;	<input type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>
The project not meeting the identified needs.	<input type="checkbox"/>

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

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Remarks: Based on a review of the National Wetlands Inventory (NWI) online mapper (<https://www.fws.gov/wetlands/data/Mapper.html>), a site visit on May 7, 2020 by United Consulting, the USGS topographic map (Appendix B, B-5) and the RFI report (Appendix E) there are six wetlands located within the 0.5 mile search radius. No wetlands are present within or adjacent to the project area. Therefore, no impacts are expected.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on May 20, 2020 with several recommendations to avoid or minimize impacts to fish, wildlife and botanical resources (Appendix C, C-3 to C-4). The USFWS responded with an email on April 28, 2020 (Appendix C, C-18 to C-19), indicating that the project is within range of the Indiana bat and Northern long-eared bat (NLEB) and should follow the programmatic consultation process in addition to the project being located within the karst area of Indiana and that their list of standard recommendations will apply to the project. All applicable IDNR DFW recommendations are included in the Environmental Commitments section of this CE document.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Unique or High Quality Habitat	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Use the remarks box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks: Based on a desktop review, a site visit on May 7, 2020 by United Consulting, and the aerial map of the project area (Appendix B, B-2) there are agricultural fields, mowed grass lawns, and scattered patches of wooded/forested land present within and adjacent to the project area.

Impacts to agricultural fields, mowed grass lawns and forested terrestrial habitat will occur as a result of the new roundabout construction. Impacts will be minimized to the greatest extent possible. It is anticipated that 0.28 acre of tree clearing will occur to provide a usable work area for construction. Mitigation to compensate for tree clearing measures has not been anticipated.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on May 20, 2020 with several recommendations to avoid or minimize impacts to fish, wildlife and botanical resources (Appendix C, C-3 to C-4). The USFWS responded with an email on April 28, 2020 (Appendix C, C-18 to C-19), indicating that the project is within range of the Indiana bat and Northern long-eared bat (NLEB) and should follow the programmatic consultation process in addition to the project being located within the karst area of Indiana and that their list of standard recommendations will apply to the project. All applicable IDNR DFW recommendations are included in the Environmental Commitments section of this CE document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst	<u>Yes</u>	<u>No</u>
Is the proposed project located within or adjacent to the potential Karst Area of Indiana?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Are karst features located within or adjacent to the footprint of the proposed project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If yes, will the project impact any of these karst features?	<input type="checkbox"/>	<input type="checkbox"/>

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Memorandum of Understanding (MOU). However, according to the topographic map of the project area (Appendix B, B-5), and the RFI report (Appendix E), and there are karst features identified within or adjacent to the project area. The identified karst features include Cave Entrance Density, Sinkhole Area, and Sinking-Stream Basin. One sinkhole area was identified approximately 0.03 mile east of

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the eastern project terminus. This project involves construction of a new roundabout at the existing intersection and will not involve any impacts to the nearest identified sinkhole area. A karst study was not required after consultation with INDOT Ecology and Waterway Permitting (EWPO) (Appendix C, C-41). In the early coordination response, the Indiana Geological Survey (IGS) did indicate that karst features may exist in the project area (Appendix C, C-7 to C-9). The IGS response letter states that geological hazards include high liquefaction potential. The IGS letter goes on to state mineral resources including moderate potential to encounter bedrock and high potential to encounter sand and gravel are present. The features will not be affected as this intersection improvement project does not involve excavation that would extend more than 2.0 feet in depth. Response from IGS has been communicated with the designer on September 25, 2020. No impacts are expected.

	Presence	Impacts	
Threatened or Endangered Species		Yes	No
Within the known range of any federal species	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Is Section 7 formal consultation required for this action?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
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Remarks: Based on a desktop review and the RFI report (Appendix E), completed by United Consulting on March 13, 2020, the IDNR Endangered, Threatened and Rare (ETR) Species List has been checked and is included in (Appendix E, E-13 to E-16). The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR DFW early coordination response letter dated May 20, 2020 (Appendix C, C-3 to C-4), the Natural Heritage Program’s Database has been checked, and to date, no plant or animal species listed as state or federally threatened, endangered or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS’s Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, C-21 to C-26). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). Additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.

The official species list generated from IPaC indicated one other species present within the project area. The Gray Bat (*Myotis grisescens*) was identified as potentially being present within the project. The project qualifies for the USFWS Interim Policy.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on January 29, 2021 and based on the responses provided, the project was found to “May Affect, Not Likely to Adversely Affect” the Indiana bat and/or the NLEB (Appendix C, C-27 to C-40). INDOT reviewed and verified the effect finding and requested USFWS’s review of the finding on January 29, 2021 (Appendix C, C-20). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

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SECTION B – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area
- Public Water System(s)
- Residential Well(s)
- Source Water Protection Area(s)
- Sole Source Aquifer (SSA)

Presence	Impacts	
	Yes	No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If a SSA is present, answer the following:

- Is the Project in the St. Joseph Aquifer System?
- Is the FHWA/EPA SSA MOU Applicable?
- Initial Groundwater Assessment Required?
- Detailed Groundwater Assessment Required?

Yes	No
<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

The project is located in Clark County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project. A detailed groundwater assessment is not needed, and no impacts are expected.

The Indiana Department of Environmental Management’s Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on November 23, 2020 by United Consulting. This project is not located within a Wellhead Protection Area or Source Water Area. In an early coordination letter dated February 5, 2021, IDEM stated the project is not located within a wellhead area or source water area (Appendix C, C-42). No impacts are expected.

The Indiana Department of Natural Resources Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on November 23, 2020 by United Consulting. No wells are located near this project. Therefore, no impacts are expected.

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by United Consulting on November 20, 2020, and the RFI report (Appendix E); this project is located in an Urban Area Boundary (UAB) location. An early coordination letter was sent on November 24, 2020 to the Clark County MS4, Storm Water Coordinator. The MS4 coordinator did not respond within the 30-day time frame. A sample of the MS4 letter has been included in Appendix C, C-43.

Based on a desktop review, a site visit on May 7, 2020 by United Consulting and the aerial map of the project area, this project is located where there is a public water system. The public water system will be affected because a section of the public water utility crossing CR 403 will have to be relocated due to its connection with a hydrant that will conflict with the roundabout. Relocation plans are being developed to move the utility within the right-of-way. Coordination with utilities is ongoing.

Flood Plains

- Longitudinal Encroachment
- Transverse Encroachment

Presence	Impacts	
	Yes	No
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

This is page 11 of 23 Project name: CR 403 at Stacy Road Intersection Improvement Date: April 19, 2021

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Project located within a regulated floodplain
 Homes located in floodplain within 1000' up/downstream from project

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks: The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on November 23, 2020 by United Consulting. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix B, B-7). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Agricultural Lands	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* 137
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: Based on a desktop review, a site visit on May 7, 2020 by United Consulting, and the aerial map of the project area (Appendix B, B-2), the project will convert 0.62 acre of farmland as defined by the Farmland Protection Policy Act. An early coordination letter was sent on April 20, 2020 to Natural Resources Conservation Service (NRCS). Coordination with NRCS resulted in a score of 137 on the AD 1006 Form (Appendix C, C-5 to C-6). NRCS's threshold score for significant impacts to farmland that result in the consideration of alternatives is 160. Since this project score is less than the threshold, no significant loss of prime, unique, statewide, or local important farmland will result from this project. No alternatives other than those previously discussed in this document will be investigated without reevaluating impacts to prime farmland.

SECTION C – CULTURAL RESOURCES

	<u>Category</u>	<u>Type</u>	<u>INDOT Approval Dates</u>	<u>N/A</u>
Minor Projects PA Clearance				

**Eligible and/or Listed
Resource Present**

Results of Research

Archaeology	
NRHP Buildings/Site(s)	
NRHP District(s)	
NRHP Bridge(s)	

Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

**Documentation
Prepared**

Documentation (mark all that apply)

	<u>ES/FHWA Approval Date(s)</u>	<u>SHPO Approval Date(s)</u>
Historic Properties Short Report		

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Historic Property Report	X	August 18, 2020	September 29, 2020
Archaeological Records Check/ Review			
Archaeological Phase Ia Survey Report	X	August 18, 2020	September 29, 2020
Archaeological Phase Ic Survey Report			
Archaeological Phase II Investigation Report			
Archaeological Phase III Data Recovery			
APE, Eligibility and Effect Determination	X	November 17, 2020	December 7, 2020
800.11 Documentation	X	November 17, 2020	December 7, 2020

MOA Signature Dates (List all signatories)

Memorandum of Agreement (MOA)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Area of Potential Effect (APE):
 The Area of Potential Effects (APE) is at least one property deep on each side of the roadway through the project area. The APE was widened in areas with greater viewshed to the project area. A map of the APE can be found in Appendix D, D-10.

Coordination with Consulting Parties:
 Per 36 CFR 800.2(c)(1) the Federal agency fulfilling the Section 106 requirements is statutorily obligated to involve stakeholders in consultation. Per Section 101 (b)(3) of the National Historic Preservation Act of 1966, the SHPO is responsible for consulting on Federal undertakings that may affect historic properties.

In addition to the SHPO, the parties listed below were invited to participate as consulting parties for this undertaking. The parties were requested to indicate whether they agreed or did not agree to participate as a consulting party within 30 days of receipt of the invitation. It was noted that if the invited consulting party did not reply, they would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed.

Invited Consulting Party	Accepted/Declined Invitation
Dr. Treva Hodges, Charlestown Mayor	No Response
Jack Coffman, Clark County Commissioner	No Response
Connie Sellers, Clark County Commissioner	No Response
Bryan Glover, Clark County Commissioner	No Response
R. Scott Lewis, Clark County Commissioner	No Response
Indiana Landmarks, Southern Regional Office	No Response
Jasper County Historical Society	No Response
Jeanne Burke, Clark County Historian/Clark County History Museum	No Response

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Donna Hart, Charlestown Architectural Preservation Society	No Response
Jarrett Haley, Kentuckiana Regional Planning and Development Agency	No Response
Eastern Shawnee Tribe of Oklahoma	No Response
Shawnee Tribe	No Response
Miami Tribe of Oklahoma	Accepted
Peoria Tribe of Indiana of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	No Response
United Keetoowah Band of Cherokee Indians	No Response
Delaware Tribe of Indians	No Response

A hard copy of the Early Coordination Letter (ECL) was sent to the SHPO on May 21, 2020, and the other consulting parties received it via email. The SHPO was requested to identify the need to include additional consulting parties.

In a letter dated June 1, 2020 the SHPO acknowledged receipt of the ECL and noted they were not aware of any further stakeholders who should be invited to be consulting parties.

In a letter dated June 4, 2020 the Miami Tribe of Oklahoma accepted the invitation to participate as a consulting party and stated that if any Native American artifacts are discovered during the project their office should be notified immediately. No other replies were received.

On September 14, 2020 a request to review the HPR and the Archaeology Report was sent to the SHPO and the consulting parties that accepted the May 21, 2020 invitation. The SHPO was provided hard copy of both reports, and the other consulting parties were notified of the availability of the HPR for review at IN SCOPE (<http://erms.indot.in.gov/Section106Documents/>). Recipients of this review request were asked to provide comments within 30 days.

In a letter dated September 29, 2020 the SHPO acknowledged receipt of the HPR and Archaeology Reports. The SHPO agreed with the HPR's proposed APE and recommendations. The SHPO concurred with the opinion of the archaeologist that no further archaeological investigations are necessary. The SHPO recommended INDOT to put forth a finding. No other replies were received.

Archaeology:

An Archaeological Phase Ia report was prepared by Metric Environmental, LLC (Snell, 8/19/20). Snell is a Qualified Professional (QP) archaeologist who meets the Secretary of the Interior's Professional Qualification Standards pursuant to 36 CFR Section 800.4(b). A literature review at the Department of Historic Preservation and Archaeology (DHPA) identified three archaeological and four site forms that have previously been recorded within 1.0 mile of the project.

On July 28, 2020, Snell and other Metric staff conducted field work that included a visual inspection, pedestrian survey, soil cores, and four shovel test probes in the project area. As a result of their investigation they determined the soil in the project area was previously disturbed by road and residential construction activity and buried utilities, and located no archaeological resources.

Snell prepared and submitted a report to the INDOT's CRO for review. The report recommended the project be allowed to proceed with no additional work. On September 14, 2020, the CRO released the

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report for submittal to the State Historic Preservation Office (SHPO) and to the appropriate Native American tribes for review and comment. The Archaeology Report Summary and Recommendations are presented in Append D, D-31 to D-33.

Historic Properties:

To determine the presence of historic properties within the project’s APE a short-format historic property report (HPR) was prepared by Metric Environmental, LLC (Hudziak, 8/3/2020). Hudziak is a Qualified Professional (QP) and meets the Secretary of the Interior’s Professional Qualification Standards pursuant to 36 CFR Section 800.4(b). Hudziak reviewed historical resources such as the Indiana Register of Historic Sites and Structures and the National Register of Historic Places (NRHP) and found no previously listed properties within the APE. Hudziak also reviewed the Clark County interim report (1988, Historic Landmarks Foundation of Indiana; resurveyed in 2010 by the Indiana Division of Historic Preservation and Archaeology), the State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBC), to determine past historical investigations conducted in the project area, and found that no previously surveyed properties were located within the APE.

Additionally, on June 30, 2020 Hudziak conducted a field survey of the project area. The HPR included the project’s APE and recommended that no properties within the APE are eligible for NRHP listing.

The Abstract and Conclusion sections of the HPR are presented in Appendix D. The INDOT Cultural Resources Office (INDOT-CRO) determined that the HPR was suitable for distribution to consulting parties on August 28, 2020.

Documentation, Findings:

No properties listed in or determined eligible for listing in the NRHP were identified within the APE. Therefore, the finding for this project is “No Historic Properties Affected”. INDOT CRO on behalf of FHWA issued a "No Historic Properties Affected" finding on November 17, 2020 (Appendix D, D-1). The SHPO concurred with the “No Historic Properties Affected” finding on December 7, 2020 (Appendix D, D-52 to D-53).

Public Involvement:

To meet the public involvement requirements of Section 106, INDOT on behalf of FHWA, advertised the finding of "No Historic Properties Affected" in the Clark County *News and Tribune* on February 10, 2021. The public comment period closed on March 12, 2021. The affidavit of publication appears on Appendix D, D-54 to D-55. One comment was received by the published deadline; however, this comment did not pertain to the Section 106 finding (Appendix D, D-56). This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

	<u>Presence</u>	<u>Use</u>	
		<u>Yes</u>	<u>No</u>
Parks & Other Recreational Land			
Publicly owned park	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Publicly owned recreation area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

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Evaluations
Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

FHWA
Approval date

--

Wildlife & Waterfowl Refuges

National Wildlife Refuge
National Natural Landmark
State Wildlife Area
State Nature Preserve

Presence

Use

Yes	No

Evaluations
Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

FHWA
Approval date

--

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

--

Use

Yes	No

Evaluations
Prepared

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

FHWA
Approval date

--

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, site visits on May 7, 2020 by United Consulting, the aerial map of the project area (Appendix B, B-2) and the RFI report (Appendix E) there are no Section 4(f) resources located within the 0.5 mile search radius. There are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

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Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the Land and Water Conservation Fund (LWCF) website at (<https://www.in.gov/indot/2523.htm>) revealed a total of 20 properties in Clark County (Appendix I, I-1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources as a result of this project.

SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

If YES, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks:

This project is included in the Fiscal Year (FY) 2020-2025 Kentuckiana Regional Planning and Development Agency Transportation Improvement Program (KIPDA TIP) (Appendix G, G-1 to G-2) and the 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix G, G-3).

The project is located in Clark County, which is currently in attainment of all criteria pollutants according to the IDEM's web page for *Nonattainment Status for Indiana Counties*, accessed at (<https://www.in.gov/idem/airquality/2339.htm>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

The project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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SECTION F - NOISE

Noise **Yes** **No**
 Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

	No	Yes/ Date
ES Review of Noise Analysis	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the community have an approved transition plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, are steps being made to advance the community's transition plan?	<input type="checkbox"/>	<input type="checkbox"/>
Does the project comply with the transition plan? (explain in the remarks box)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Remarks:

The preferred alternative is consistent with the Clark County Comprehensive Plan and local land use plans developed for the Clark County. No negative impacts to community cohesion are anticipated. As a result, the project is expected to comply with the local/regional development patterns for the area. No negative impacts to community cohesion are anticipated. This project will not have any significant short or long-term economic impacts. There are no parks or community buildings near the project. As a result, no impacts to community events are expected. No pedestrian facilities exist within the project vicinity. As a result, the project complies with the approved American with Disabilities Act (ADA) transition plan for the Clark County, Indiana.

Indirect and Cumulative Impacts **Yes** **No**
 Will the proposed action result in substantial indirect or cumulative impacts?

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance, but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such actions.

 There are no substantial indirect or cumulative effects resulting from the project. The proposed project addresses safety and vehicular mobility concerns across the intersection of CR 403 and Stacy Road that are projected to grow worse if not addressed. The project is not designed for and therefore will not induce growth beyond that reasonably expected, based on current growth rates. It will not provide access to currently

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inaccessible areas that could experience changes in land use patterns. Incremental impacts to natural resources such as threatened and endangered species are addressed by the environmental commitments proposed for the project.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes **No**

Remarks: Based on a desktop review, a site visit on May 7, 2020 by United Consulting, the aerial map of the project area (Appendix B, B-2) and the RFI report (Appendix E) there is one public facility (church) within the 0.5 mile search radius. Pleasant Grove Methodist Church is located at the southwest quadrant of the intersection between CR 403 and Stacy Road. This intersection improvement project has been designed so that no loss of parking spaces occurs to the Pleasant Grove Methodist Church property. Access to all properties will be maintained during construction. Therefore, no impacts are expected.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified? **Yes** **No**

Does the project require an EJ analysis? **Yes** **No**

If YES, then:

Are any EJ populations located within the project area? **Yes** **No**

Will the project result in adversely high or disproportionate impacts to EJ populations? **Yes** **No**

Remarks: Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require approximately 2.30 acres of permanent ROW acquisition. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference populations may be a county, city or town and is called the community of comparison (COC). In this project, the COC is Clark County. The community that overlaps the project area is called the affected community (AC). In this project, the AC is Clark County Census Tract 509.04. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority is 125% of the COC. Data from the 2015-2019 American Community Survey 5-Year Estimates was obtained from the US Census Bureau website (<https://data.census.gov/cedsci/>) on December 10, 2020 by United Consulting. The data collected for minority and low-income populations within the AC are summarized in the table below:

Table: Minority and Low-Income Data (2019 US Census Bureau)		
	Clark County, Indiana (COC)	Census Tract 509.04 (AC)
Percent Minority	16.8%	9.2%
125% of COC	21.0%	AC < 125% of COC
EJ Population of Concern		No
Percent Low-Income	10.1%	5.9%
125% of COC	12.6%	AC < 125% of COC
EJ Population of Concern		No

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AC, Census Tract 509.04 has a percent minority of 9.2% which is below 50% and is below the 125% COC threshold. Therefore the AC does not contain minority populations of EJ concern.

AC, Census Tract 509.04 has a percent low-income of 5.9% which is below 50% and is below the 125% COC threshold. Therefore, the AC does not contain low-income populations of EJ concern.

The census data sheets, map, and calculations can be found in Appendix H. No further EJ analysis is warranted.

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
 Is a Business Information Survey (BIS) required?
 Is a Conceptual Stage Relocation Study (CSRS) required?
 Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will take place as a result of this project.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation		<input checked="" type="checkbox"/>
Phase I Environmental Site Assessment (Phase I ESA)		<input type="checkbox"/>
Phase II Environmental Site Assessment (Phase II ESA)		<input type="checkbox"/>
Design/Specifications for Remediation required?		<input type="checkbox"/>

Documentation

	No	Yes/ Date
ES Review of Investigations		X / March 15, 2020

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, an RFI was completed on March 13, 2020 by United Consulting and approved by INDOT SAM on March 15, 2020. (Appendix E). One underground storage tank (UST) site, one solid waste landfill site, and one institutional control site are located within 0.5 mile of the project area; however no hazmat sites were identified in or within 0.5 mile of the project area that will impact the project. The UST site is located 0.10 mile southwest of the project area. The solid waste landfill site is located 0.49 mile southwest of the project area. The institutional control site is the same facility as the solid waste landfill and is located 0.49 mile southwest of the project area. Further investigation for hazardous material concerns is not required at this time.

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SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)	
Nationwide Permit (NWP)	
Regional General Permit (RGP)	
Pre-Construction Notification (PCN)	
Other	
Wetland Mitigation required	
Stream Mitigation required	

IDEM

Section 401 WQC	
Isolated Wetlands determination	
Rule 5	X
Other	
Wetland Mitigation required	
Stream Mitigation required	

IDNR

Construction in a Floodway	
Navigable Waterway Permit	
Lake Preservation Permit	
Other	
Mitigation Required	

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the remarks box below)

--

Remarks:

IDEM – Rule 5 Erosion Control Permit:
As a result of ground disturbance exceeding 1.0 acre, a Rule 5 Permit from the IDEM will be required.

Applicable recommendations provided by IDNR are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

FIRM:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)

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3. Any work in a wetland area within existing right-of-way or in a borrow/waste area is prohibited unless specifically allowed in the US Army Corps of Engineers or IDEM permit. (INDOT ESD)
4. GENERAL AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
5. TREE REMOVAL AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
6. TREE REMOVAL AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed (No tree clearing April 1 - September 30). (USFWS)
7. TREE REMOVAL AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
8. TREE REMOVAL AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
9. LIGHTING AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
10. LIGHTING AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
11. HIBERNACULA AMM 1: For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography. (USFWS)

For Further Consideration:

12. Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees). (IDNR DFW)
13. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting from April 1 through September 30. [RSP 107-B-040] (IDNR DFW)

Indiana Department of Transportation

County Clark County Route CR 403/Stacy Road Des. No. 1802805

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Early Coordination			
Recipients	Date Sent	Response	Date Received
Natural Resources Conservation Service	April 20, 2020	Yes	May 7, 2020
Indiana Department of Environmental Management	April 20, 2020	Yes	January 19, 2021
U.S. Fish and Wildlife Service	April 20, 2020	Yes	April 28, 2020
U.S. Army Corps of Engineers	April 20, 2020	No	N/A
Indiana Department of Natural Resources – Division of Fish and Wildlife	April 20, 2020	Yes	May 20, 2020
INDOT Aeronautics Division	April 20, 2020	Yes	April 21, 2020
Housing and Urban Development	April 20, 2020	No	N/A
Indiana Geological Survey	April 20, 2020	Yes	April 21, 2020
Kentuckiana Regional Planning and Development Agency	April 20, 2020	No	N/A
Pleasant Grove Methodist Church	April 20, 2020	No	N/A
Clark County Highway Department	April 20, 2020	No	N/A
Clark County Board of Commissioners	April 20, 2020	No	N/A
Aggrock Quarry	April 20, 2020	No	N/A

Index to Appendix

(Des. No.: 1802805)

Appendix A: INDOT Supporting Documentation

A-1 Categorical Exclusion Level Threshold Chart

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B-2 Aerial Photography Map
B-3 Zoomed-In Aerial Photography Map
B-4 LiDAR Map
B-5 USGS Topographic Map (1:24,000)
B-6 Zoomed-In Topographic Map (1:2,500)
B-7 Flood Insurance Rate Map
B-8 National Wetlands Inventory Map
B-9 Photograph Orientation Map
B-10– B-15 Ground Level Photography
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C-18 – C-19 USFWS Early Coordination Email Response
C-20 INDOT Seymour District IPaC Verification Email
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C-27 – C-40 USFWS Concurrence Verification Letter
C-41 INDOT EWPO Karst Feature Email Coordination
C-42 IDEM Groundwater Section Early Coordination Response Letter
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Appendix D: Section 106

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D-52 – D-53 SHPO Concurrence Letter
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Appendix E: Red Flag and Hazardous Materials

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Appendix F: Public Involvement

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Appendix G: Air Quality

G-1 – G-2 2020-2025 KIPDA TIP Printouts
G-3 2020-2024 INDOT STIP Printout

Appendix H: Environmental Justice

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Appendix I: Additional Studies

I-1 Land and Water Conservation Fund Grant Listing for Clark County

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	"No Effect", "Not likely to Adversely Affect" (Without AMMs ⁴ or with AMMs required for all projects ⁵)	"Not likely to Adversely Affect" (With any other AMMs)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	"No Effect", "Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes	Yes
<ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA 				Yes	Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as "required for all projects".

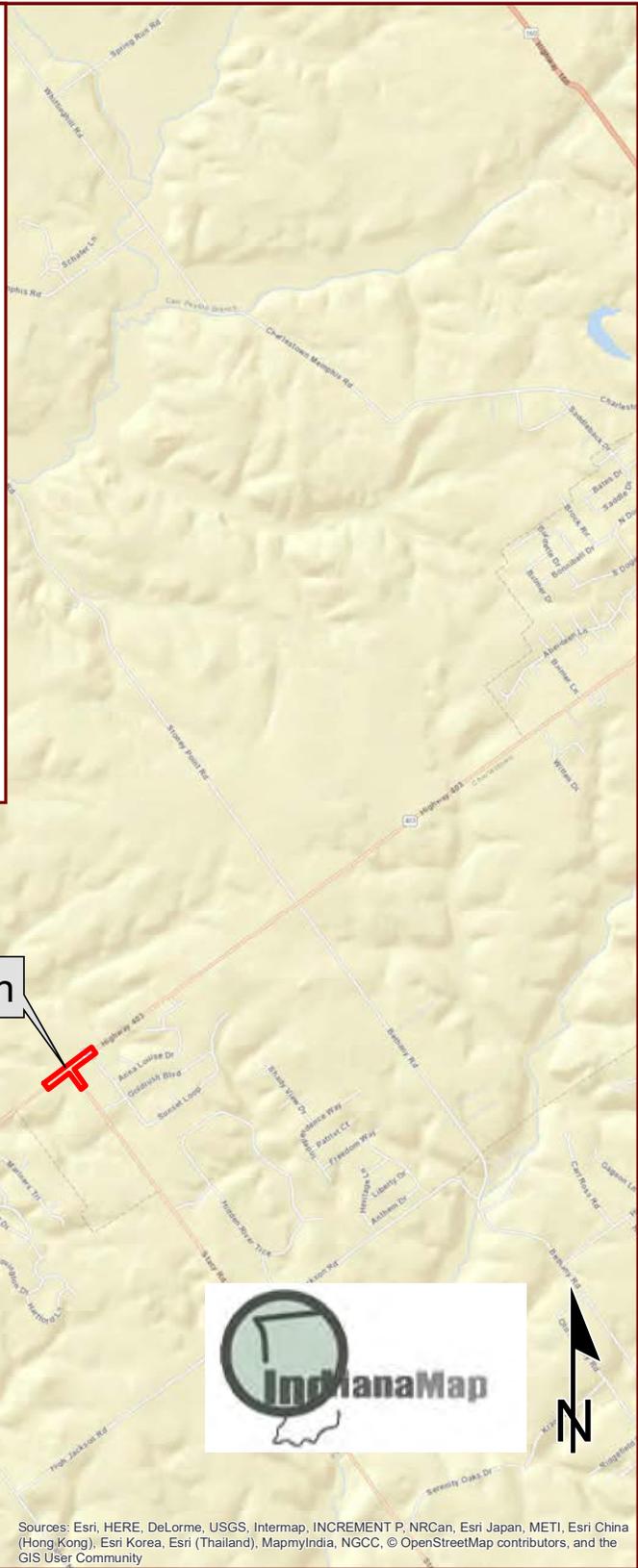
⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Appendix B

Graphics



Sources: Esri, HERE, DeLorme, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), MapmyIndia, NGCC, © OpenStreetMap contributors, and the GIS User Community

Exhibit 1 - State Location Map
 County Road 403 at Stacy Road
 Intersection Improvement Project
 Clark County, Indiana
 Des. No.: 1802805

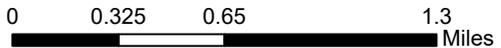




Exhibit 2 - Aerial Photography Map
 County Road 403 at Stacy Road
 Intersection Improvement Project
 Clark County, Indiana
 Des. No.: 1802805





Exhibit 3 - Zoomed-In Aerial Photography Map
County Road 403 at Stacy Road Intersection
Improvement Project
Clark County, Indiana
Des. No.: 1802805



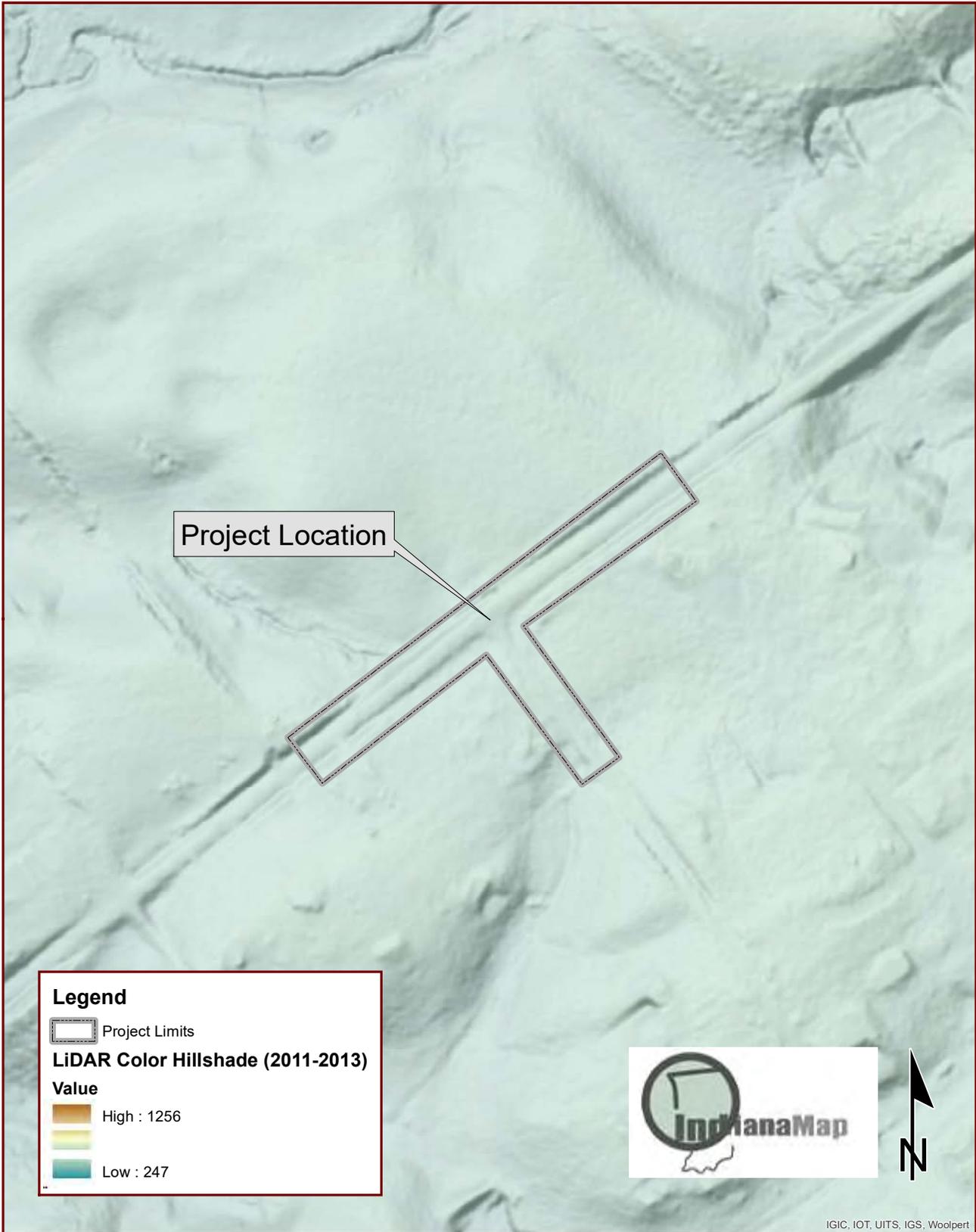


Exhibit 4 - LiDAR Elevation Map
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 Intersection Improvement Project
 Clark County, Indiana
 Des. No.: 1802805



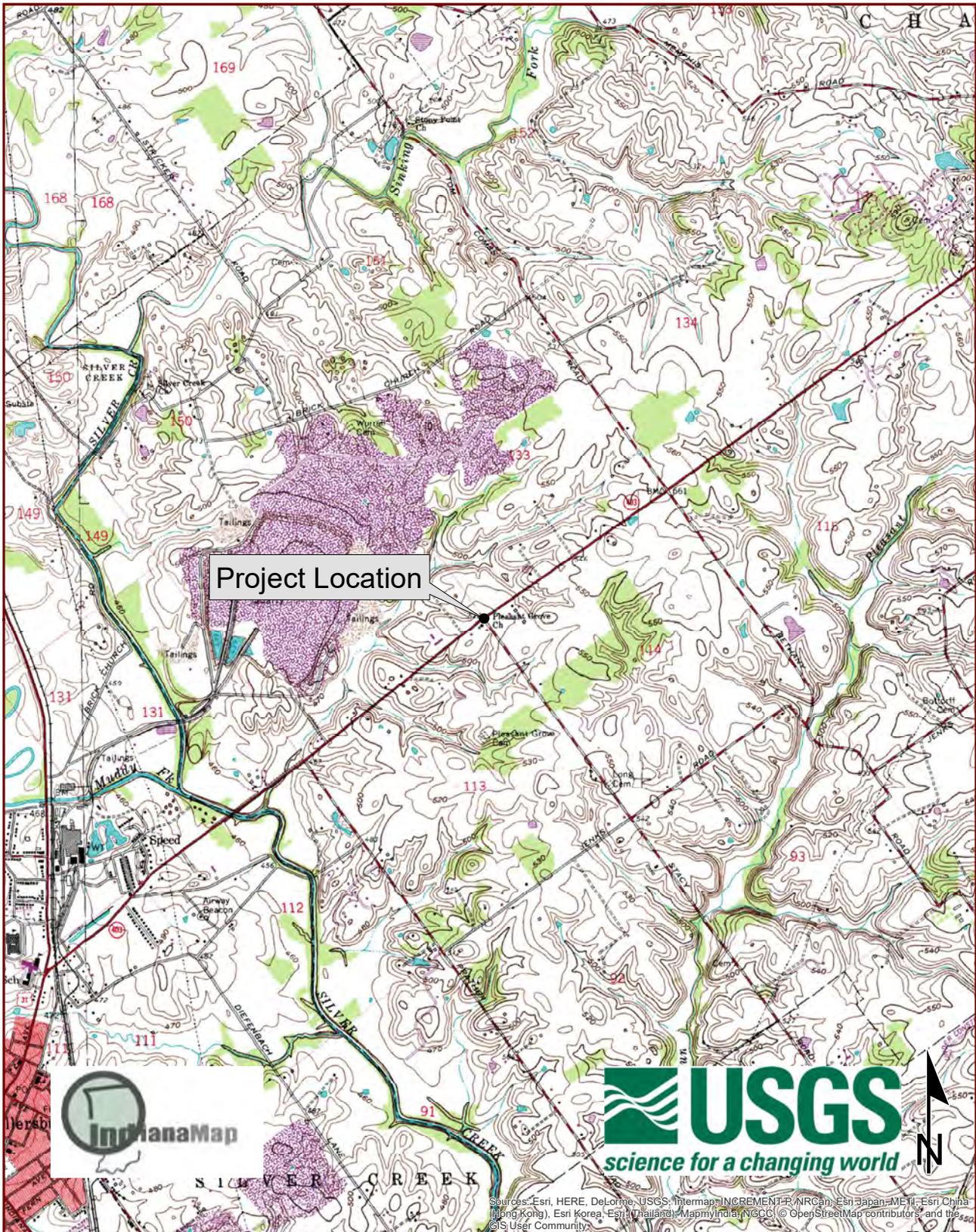


Exhibit 5 - USGS Topographic Map
 County Road 403 at Stacy Road
 Intersection Improvement Project
 Clark County, Indiana
 Des. No.: 1802805



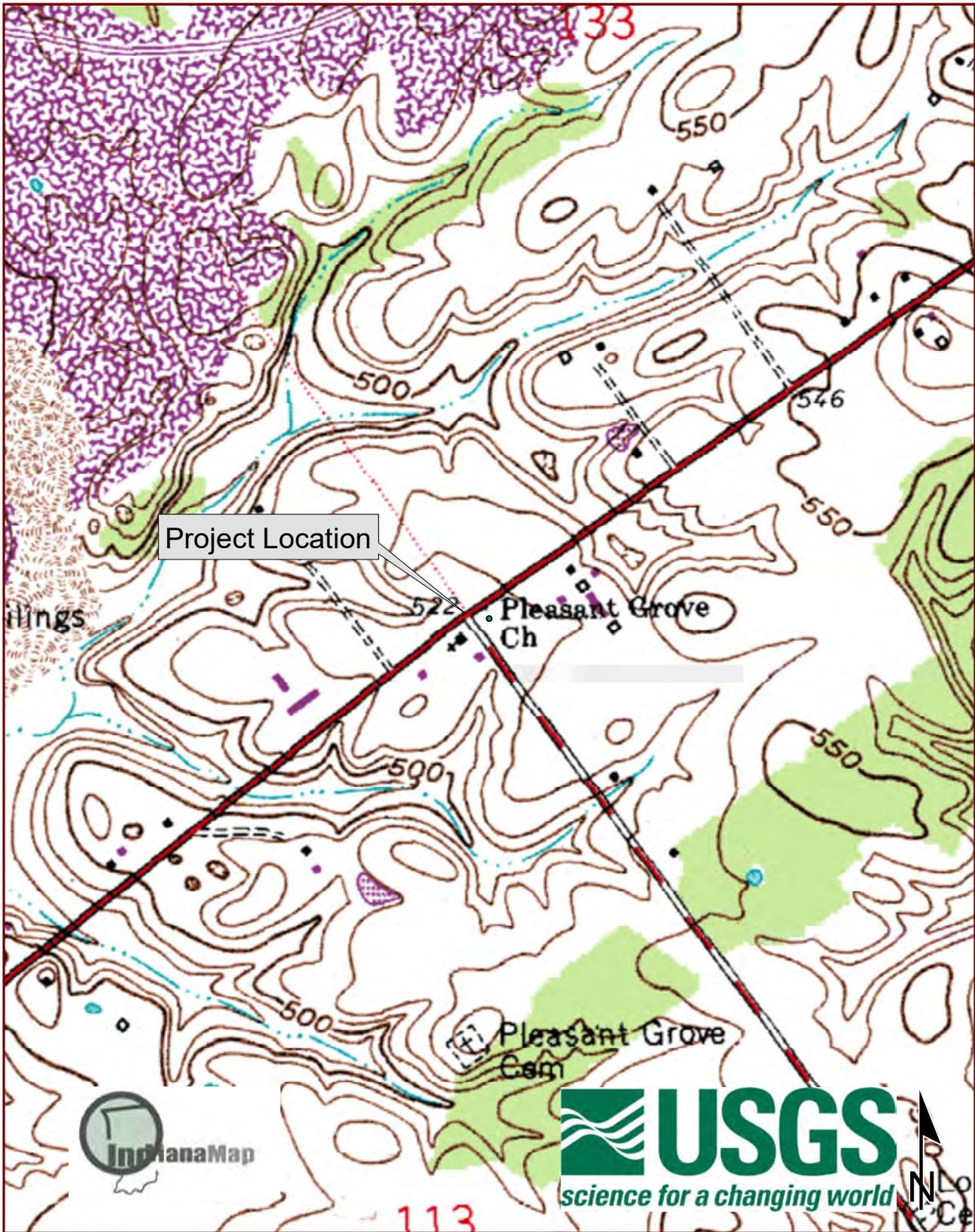
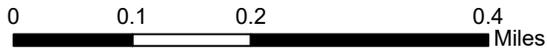


Exhibit 6 - Zoomed-in USGS Topographic Map
 County Road 403 at Stacy Road Intersection
 Improvement Project
 Clark County, Indiana
 Des. No.: 1802805



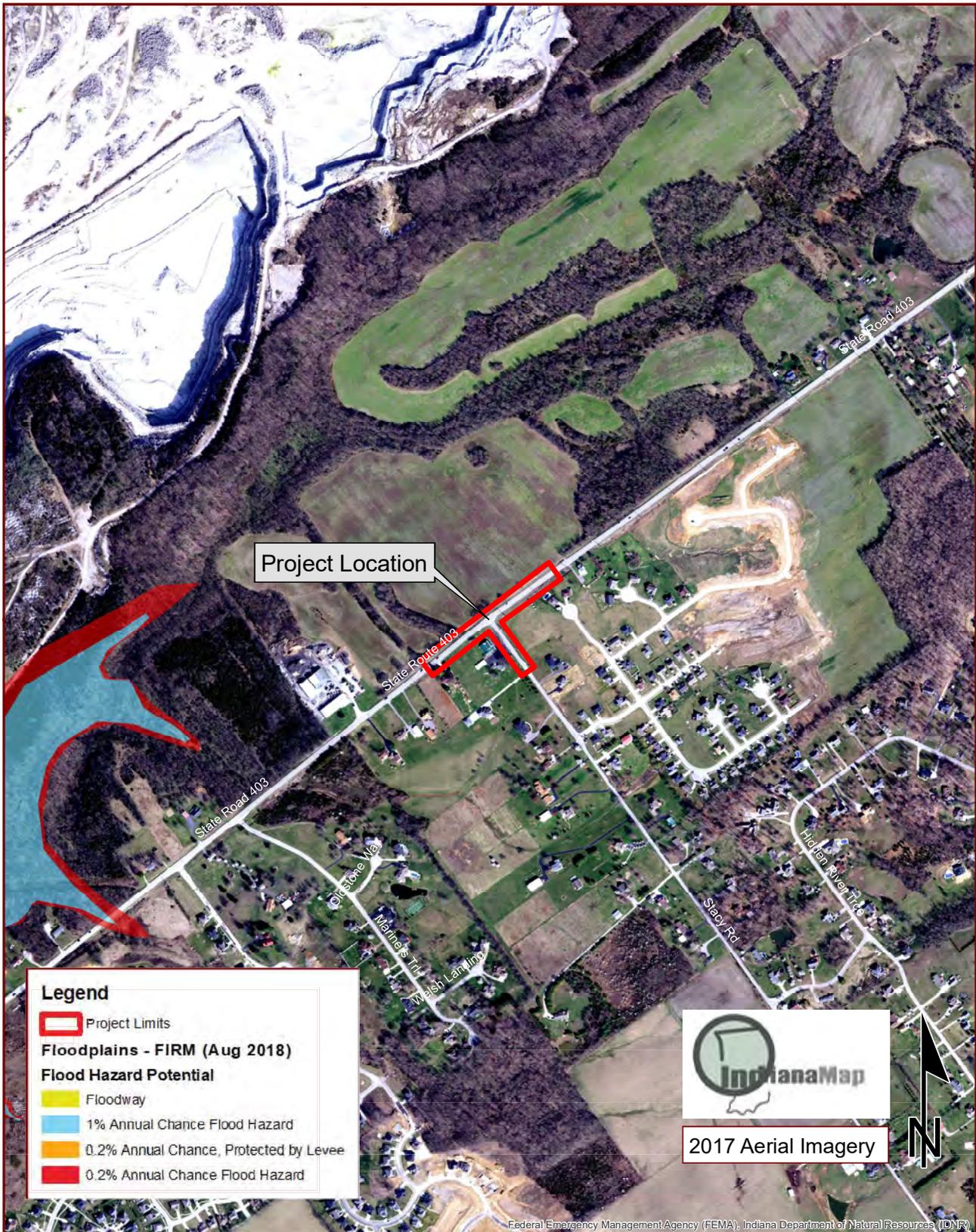
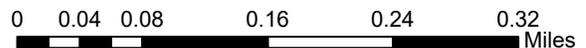


Exhibit 7 - Flood Insurance Rate Map
 County Road 403 at Stacy Road
 Intersection Improvement Project
 Clark County, Indiana
 Des. No.: 1802805





Exhibit 8 - National Wetlands Inventory Map
 County Road 403 at Stacy Road Intersection
 Improvement Project
 Clark County, Indiana
 Des. No.: 1802805



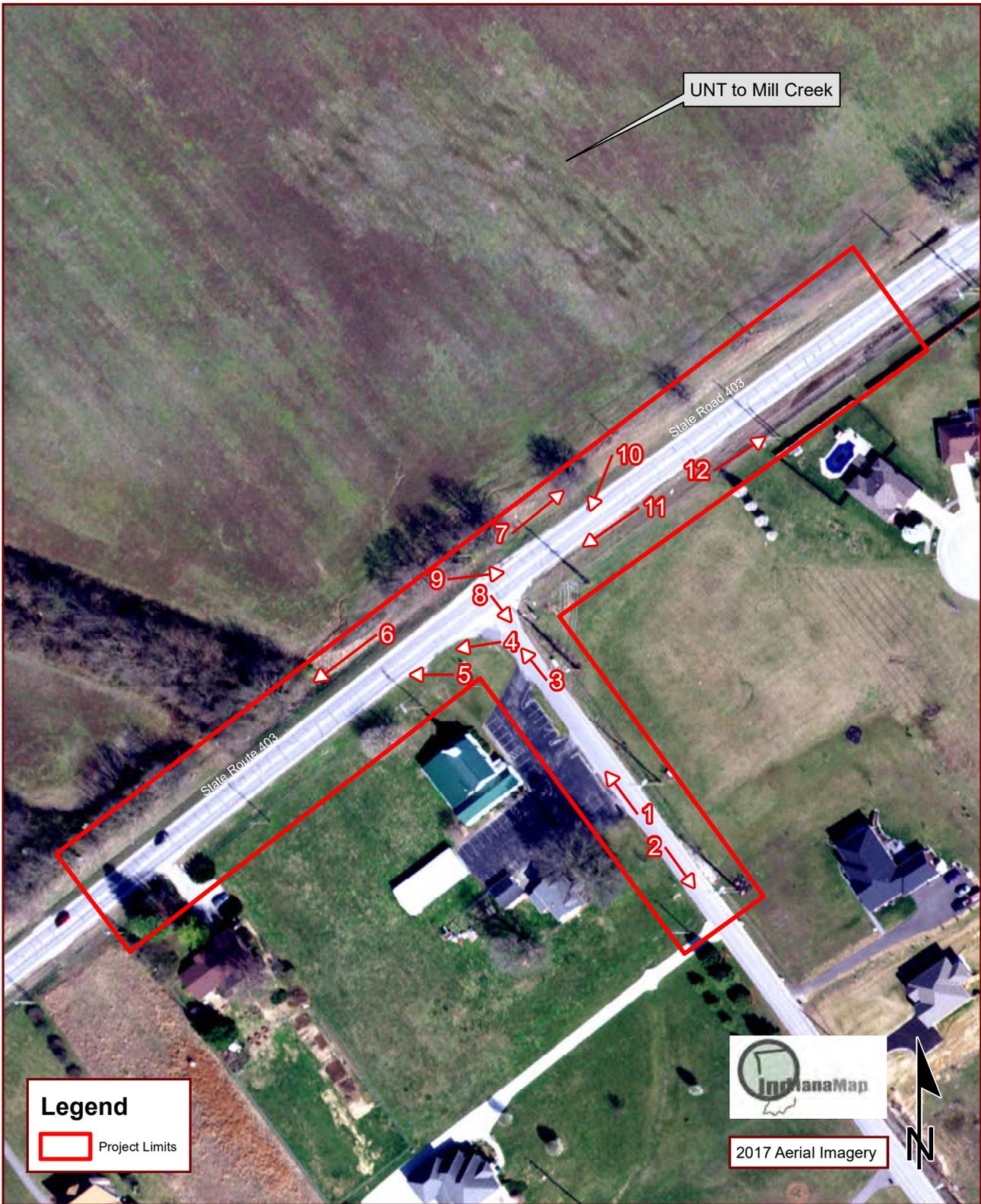
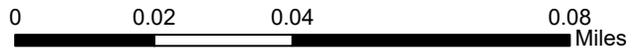


Exhibit 9 - Photograph Orientation Map
 County Road 403 at Stacy Road
 Intersection Improvement Project Clark
 County, Indiana
 Des. No.: 1802805



**County Road 403 at Stacy Road
Intersection Improvement Project
Clark County, Indiana**



Photograph #1: Looking northwest along Stacy Road near end of project limits.



Photograph #2: Looking southeast along Stacy Road toward end of project limits.

**County Road 403 at Stacy Road
Intersection Improvement Project
Clark County, Indiana**



Photograph #3: Looking northwest along Stacy Road toward County Road 403.



Photograph #4: Looking west toward County Road 403 from Stacy Road.

**County Road 403 at Stacy Road
Intersection Improvement Project**
Clark County, Indiana



Photograph #5: Looking west toward County Road 403 from Stacy Road.



Photograph #6: Looking southwest along County Road 403.

**County Road 403 at Stacy Road
Intersection Improvement Project**
Clark County, Indiana



Photograph #7: Looking northeast along County Road 403.



Photograph #8: Looking southeast toward Stacy Road from County Road 403.

**County Road 403 at Stacy Road
Intersection Improvement Project
Clark County, Indiana**



Photograph #9: Looking east across intersection of County Road 403 and Stacy Road.



Photograph #10: Looking west across intersection of County Road 403 and Stacy Road.

**County Road 403 at Stacy Road
Intersection Improvement Project
Clark County, Indiana**



Photograph #11: Looking southwest along County Road 403 toward Stacy Road.



Photograph #12: Looking northeast along County Road 403 near east end of project.

PROJECT	DESIGNATION
1802805	1802805
CONTRACT	BRIDGE FILE
N/A	N/A

INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

CR 403 AND STACY ROAD INTERSECTION IMPROVEMENT

PROJECT NO. 1802805 P.E.
1802805 R/W
1802805 CONST.

THE INTERSECTION OF CR 403 & STACY ROAD WILL BE RECONSTRUCTED.
THE PROJECT IS LOCATED IN SECTION 113, 114, 132 AND 133 CHARLESTOWN TOWNSHIP, CLARK COUNTY, INDIANA

TRAFFIC DATA	COUNTY ROAD 403	STACY ROAD
A.D.T. (2022)	12,943 V.P.D.	1,777 V.P.D.
A.D.T. (2042)	19,614 V.P.D.	2,693 V.P.D.
D.H.V. (2042)	984 V.P.H.	147 V.P.H.
DIRECTIONAL DISTRIBUTION	50%	50%
TRUCKS	5% A.A.D.T.	5% A.A.D.T.
DESIGN DATA		
DESIGN SPEED	50 M.P.H.	30 M.P.H.
PROJECT DESIGN CRITERIA	4R (NON-FREEWAY)	4R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL	LOCAL ROAD/LOCAL STREET
RURAL/URBAN	URBAN (SUBURBAN)	URBAN (SUBURBAN)
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	NONE	NONE



LATITUDE: 40°45'00.00"N LONGITUDE: -86°01'24.02"W

Gross Length: 0.170 MI.
Net Length: 0.170 MI.
Max Grade: 2.63 %

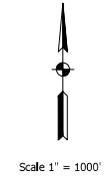
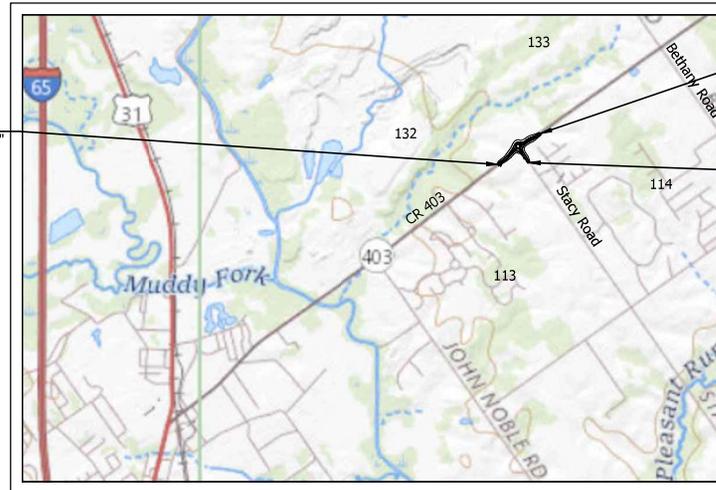
STAGE 2 PLANS
JANUARY 31, 2021

CLARK COUNTY BOARD OF COMMISSIONERS

BEGIN PROJECT
STA. 10+20.00 "PR-B"

END PROJECT
STA. 19+18.52 "PR-B"

BEGIN CONSTRUCTION
STA. 10+50.00 "PR-S-1-B"



LOCATION MAP
CLARK COUNTY

[INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020 TO BE
USED WITH THESE PLANS]

APPROVED:

JACK COFFMAN, PRESIDENT DATE

KONNIE SELLERS, MEMBER DATE

BRYAN GLOVER, MEMBER DATE

COUNTY ENGINEER

BRYAN DIXON, P.E. DATE



8440 Allison Pointe Boulevard, Suite 200
Indianapolis, IN 46250
Phone 317-895-2585
www.ucindy.com

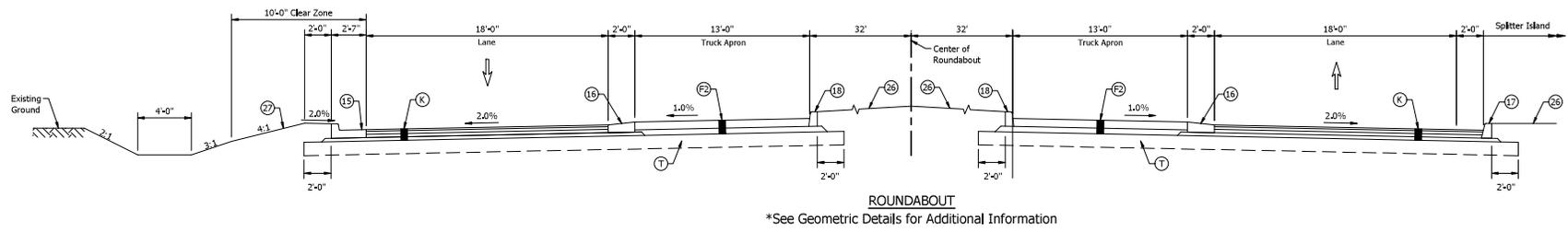
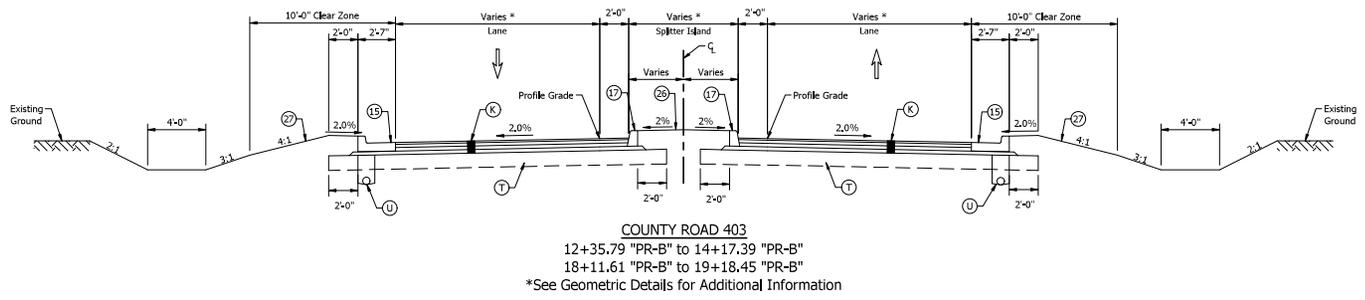
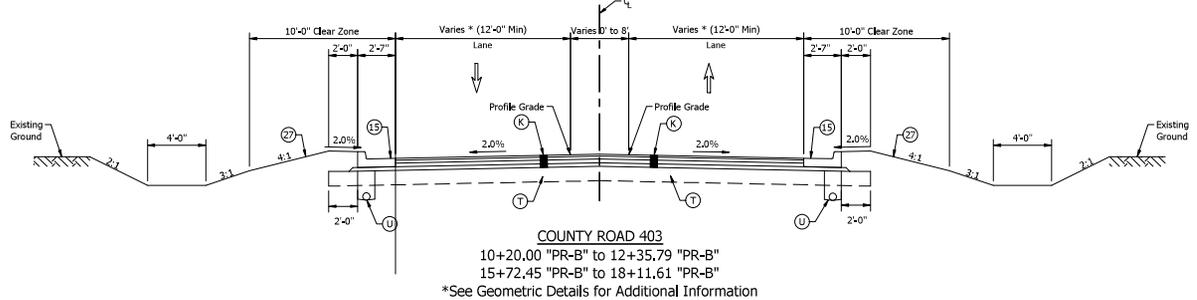
PLANS PREPARED BY: **UNITED CONSULTING** (317) 895-2585
PHONE NUMBER

CERTIFIED BY: _____ DATE

APPROVED FOR LETTING: _____ INDIANA DEPARTMENT OF TRANSPORTATION DATE

BRIDGE FILE
N/A
DESIGNATION
1802805

SURVEY BOOK	SHEETS
N/A	1 of 28
CONTRACT	PROJECT
N/A	1802805



- Ⓢ Truck Apron, consisting of:
 7" PCCP, on
 7" Compacted Aggregate, No. 53
- Ⓢ Full Depth HMA, consisting of:
 165 #/Sys QC/QA-HMA, 3, 76, Surface, 9.5 mm on
 275 #/Sys QC/QA-HMA, 2, 76, Intermediate, 19.0 mm on
 330 #/Sys QC/QA-HMA, 3, 64, Base, 19.0 mm on
 330 #/Sys QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm
- Ⓢ Curb & Gutter, Concrete, Modified
- Ⓢ Curb & Gutter, B, Concrete, Modified
- Ⓢ Curb, Concrete, Modified
- Ⓢ Curb, Integral, Concrete
- Ⓢ Center Curb, D, Concrete
- Ⓢ Nursery Sodding
- Ⓢ Seed Mixture, U
- Ⓢ Subgrade Treatment
- Ⓢ 6" Underdrain

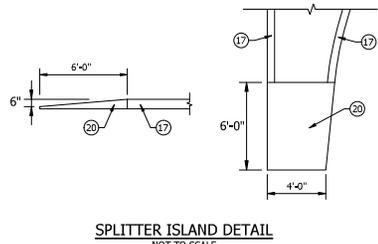
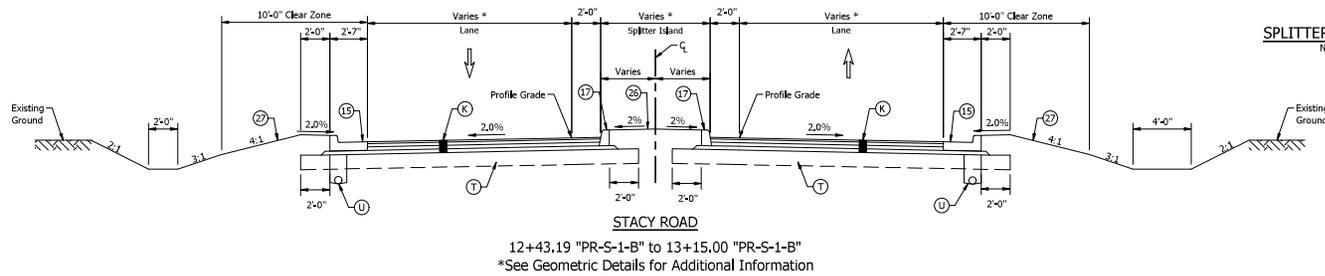
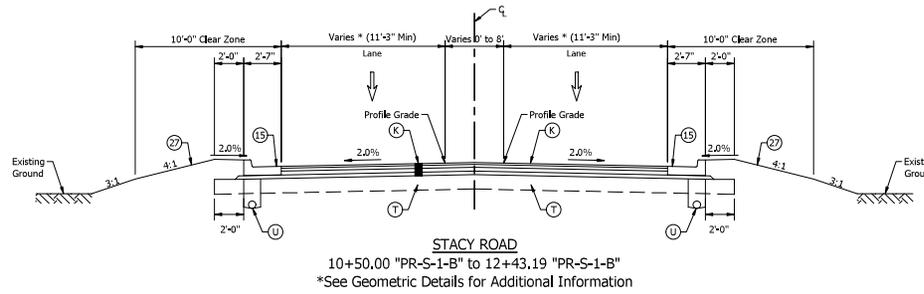
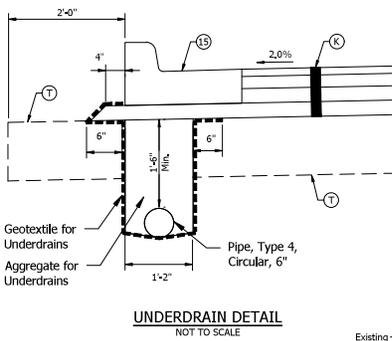
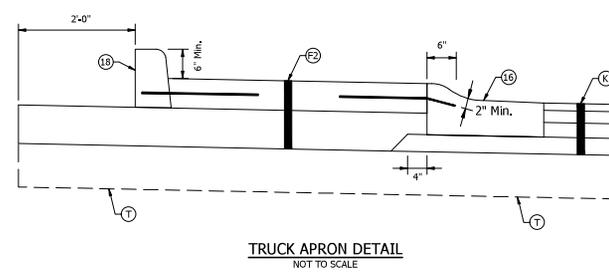
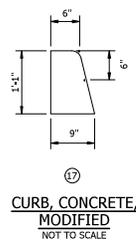
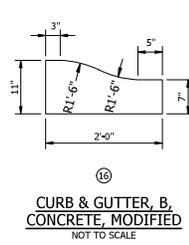
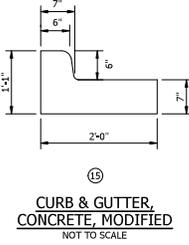
UNITED Consulting
 8440 Allison Pointe Boulevard, Suite 200
 Indianapolis, IN 46250
 Phone 317-895-2585
 www.ucindy.com

RECOMMENDED FOR APPROVAL _____
 DESIGN ENGINEER DATE _____
 DESIGNED: ESH DRAWN: VAD
 CHECKED: JEL CHECKED: ESH

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

HORIZONTAL SCALE		BRIDGE FILE	
1" = 4'	N/A	N/A	N/A
VERTICAL SCALE	N/A	DESIGNATION	1802805
SURVEY BOOK	N/A	SHEETS	1 of 1 28
CONTRACT	N/A	PROJECT	1802805
			1802805



- Ⓒ Truck Apron, consisting of:
7" PCCP, on
7" Compacted Aggregate, No. 53
- Ⓒ Full Depth HMA, consisting of:
165 #/Sys QC/QA-HMA, 3, 76, Surface, 9.5 mm on
275 #/Sys QC/QA-HMA, 2, 76, Intermediate, 19.0 mm on
330 #/Sys QC/QA-HMA, 3, 64, Base, 19.0 mm on
330 #/Sys QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm
- Ⓔ Curb & Gutter, Concrete, Modified
- Ⓕ Curb & Gutter, B, Concrete, Modified
- Ⓖ Curb, Concrete, Modified
- Ⓗ Curb, Integral, Concrete
- Ⓘ Center Curb, D, Concrete
- Ⓙ Nursery Sodding
- Ⓚ Seed Mixture, U
- Ⓛ Subgrade Treatment
- Ⓜ 6" Underdrain



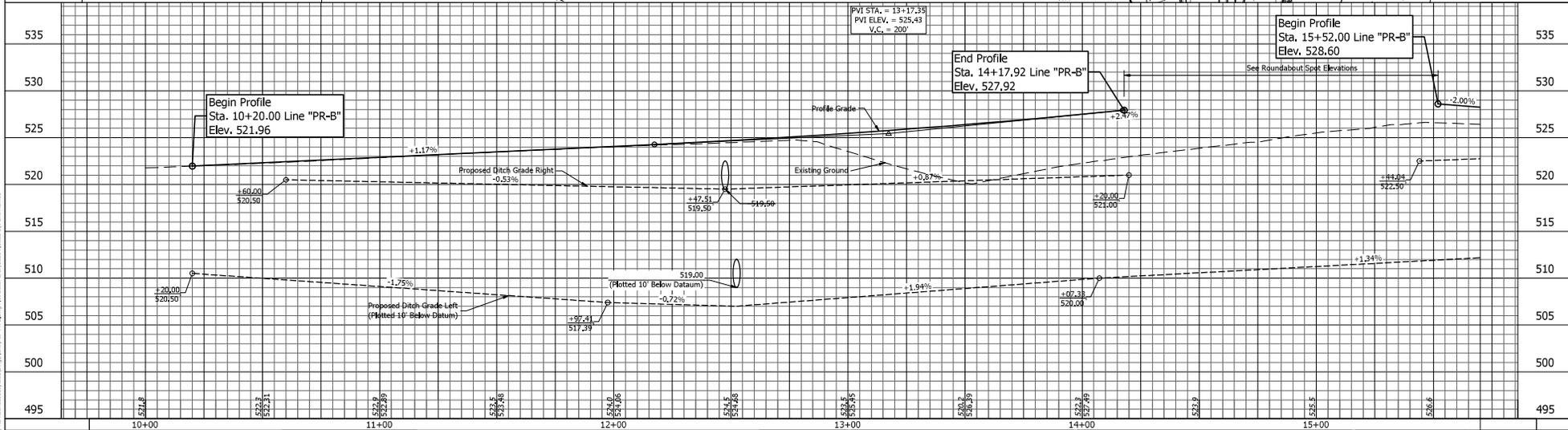
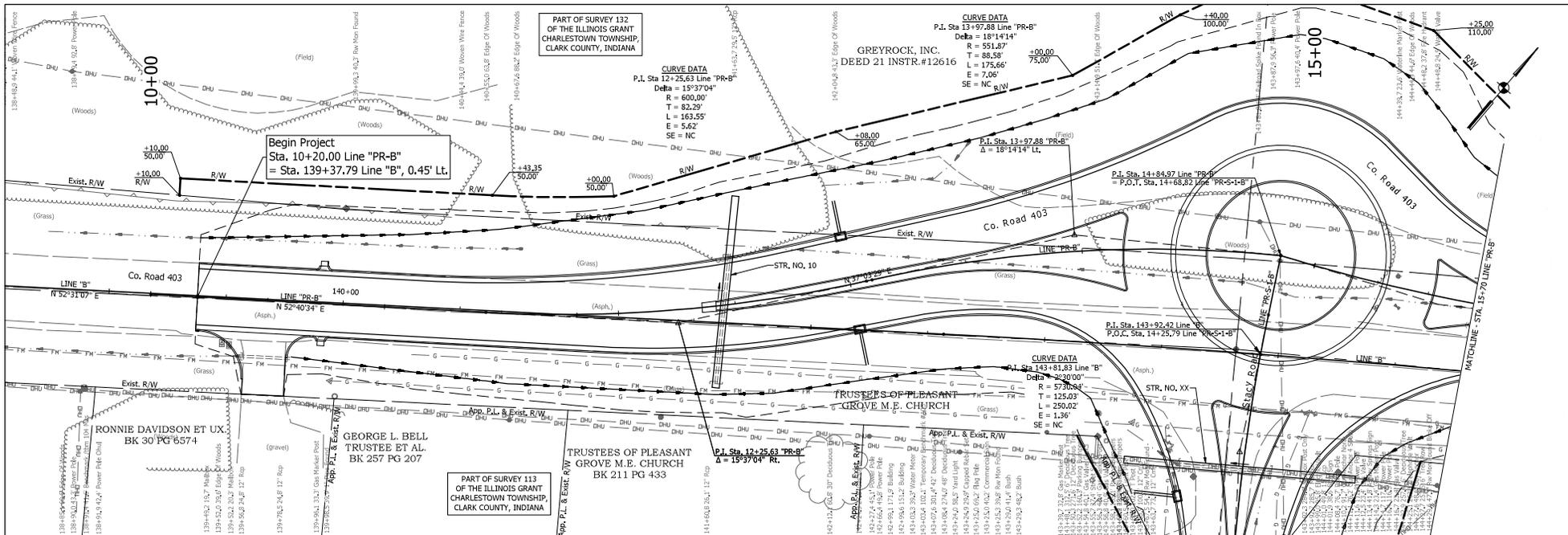
8440 Allison Pointe Boulevard, Suite 200
Indianapolis, IN 46250
Phone 317-895-2585
www.ucindy.com

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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CHECKED: JEL	CHECKED: ESH	

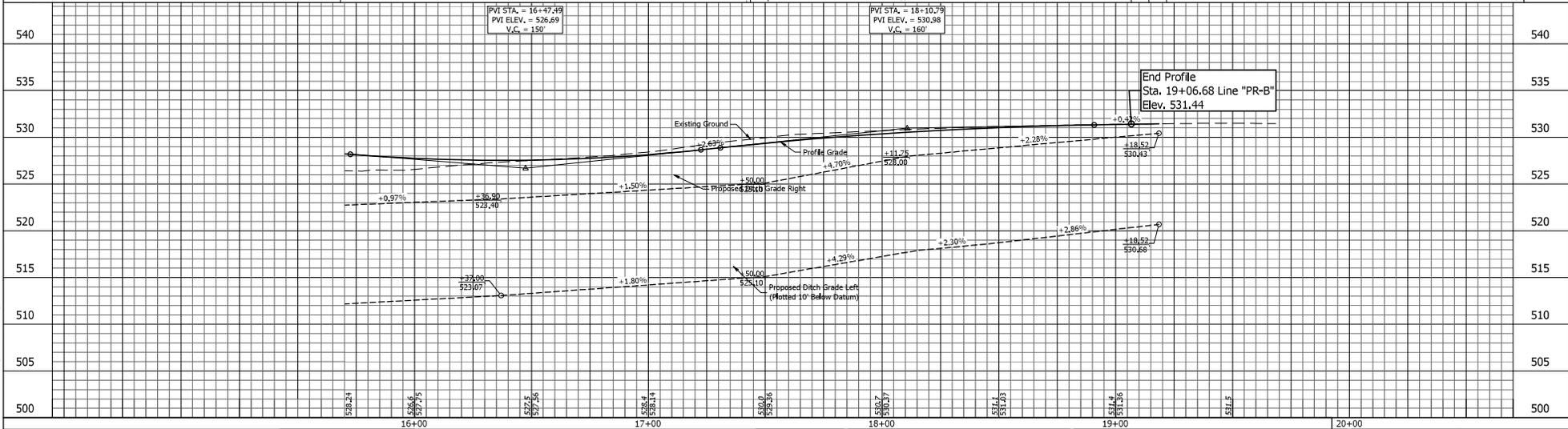
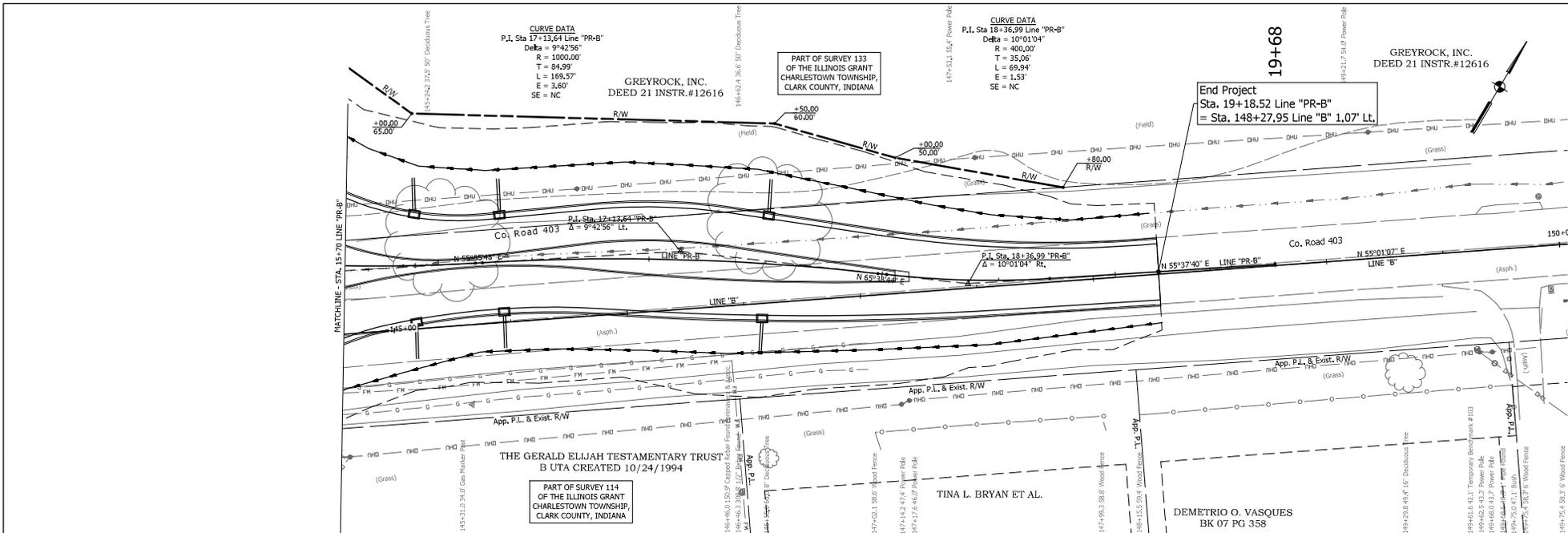
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

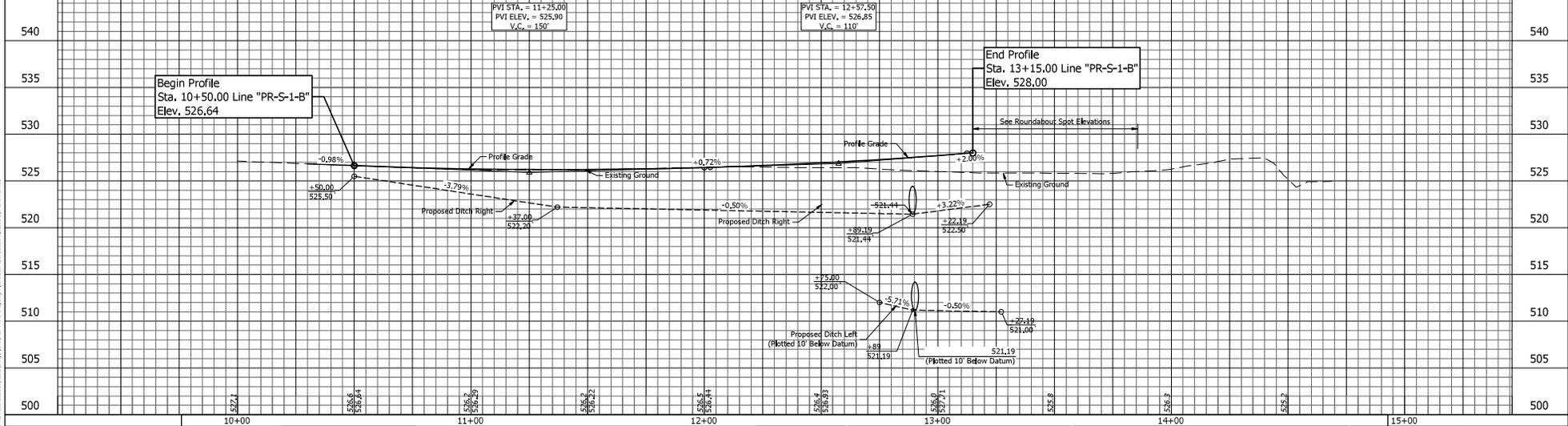
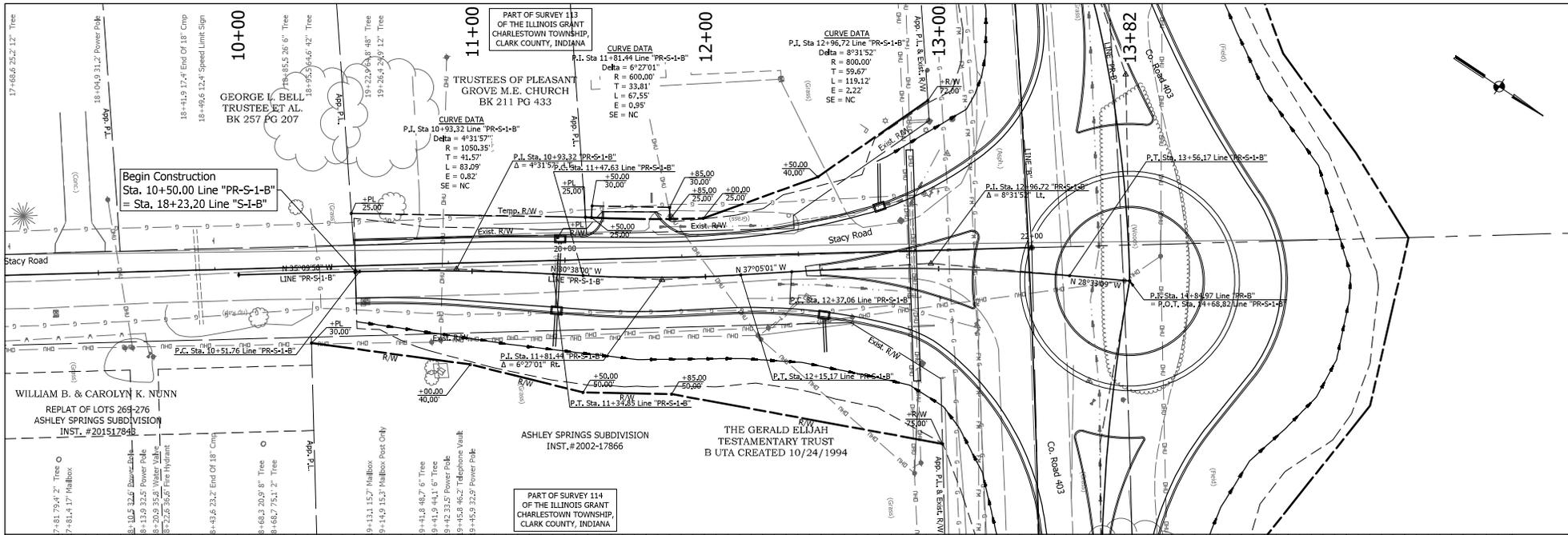
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1" = 4'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1802805
SURVEY BOOK	SHEETS
N/A	4 of 1 28
CONTRACT	PROJECT
N/A	1802805



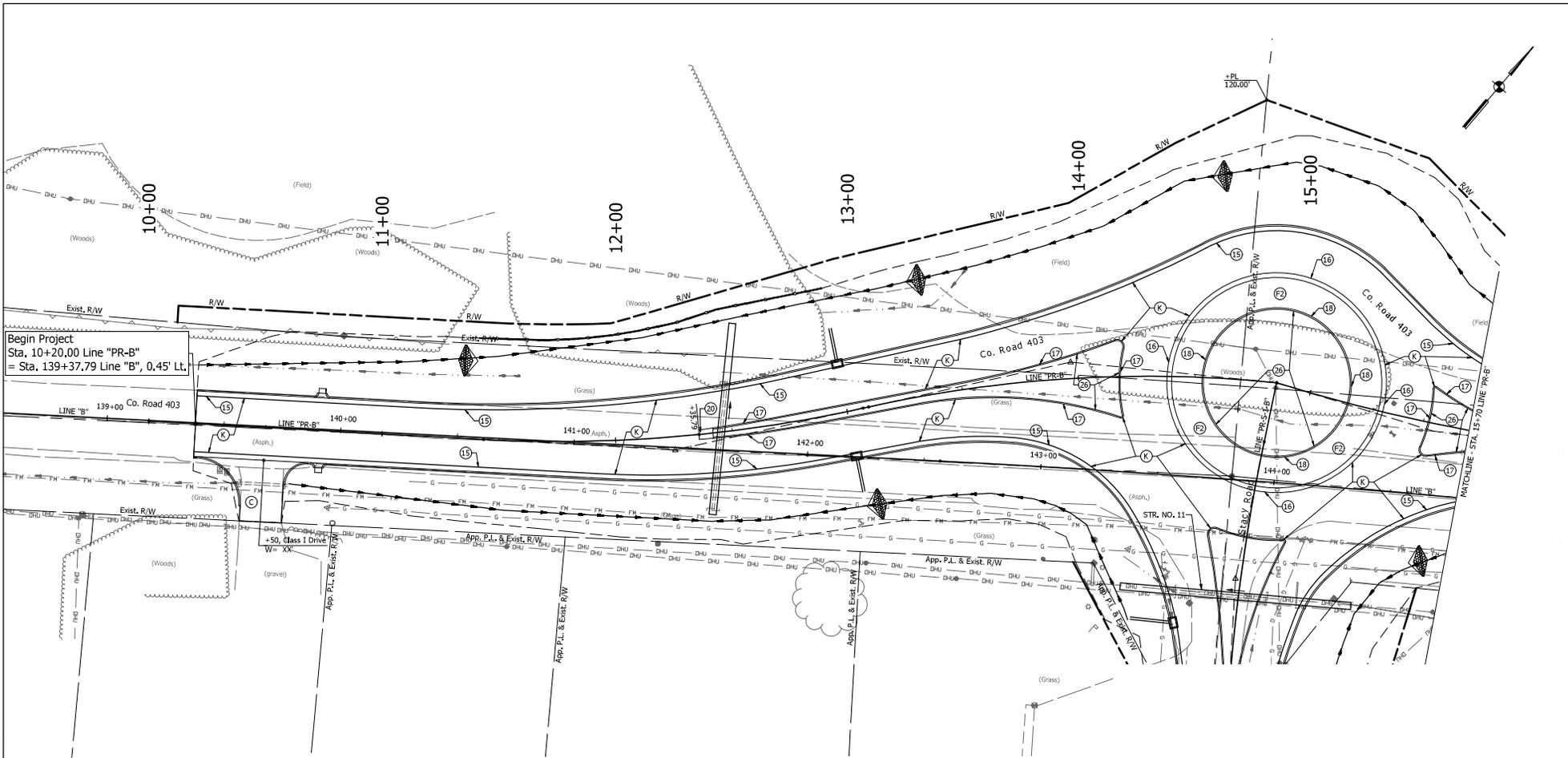
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		CHECKED: JEL CHECKED: ESH	SURVEY BOOK N/A CONTRACT N/A	SHEETS 7 OF 1 PROJECT 1802805	
		INDIANA DEPARTMENT OF TRANSPORTATION		PLAN & PROFILE	
		LINE "PR-B" (COUNTY ROAD 403)		LINE "PR-B" (COUNTY ROAD 403)	



	8440 Allison Pointe Boulevard, Suite 200 Indianapolis, IN 46250 Phone 317-895-2585 www.ucindy.com	RECOMMENDED FOR APPROVAL _____ DATE _____ DESIGN ENGINEER	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 20'	BRIDGE FILE N/A
		DESIGNED: ESH CHECKED: JEL		DRAWN: VAD CHECKED: ESH	VERTICAL SCALE 1" = 5'
		PLAN & PROFILE LINE "PR-B" (COUNTY ROAD 403)		SURVEY BOOK N/A	SHEETS 8 OF 28
				CONTRACT N/A	PROJECT 1802805

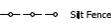


	8440 Allison Pointe Boulevard, Suite 200 Indianapolis, IN 46250 Phone 317-895-2585 www.ucindy.com	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE _____ DESIGNED: ESH DRAWN: VAD CHECKED: JEL CHECKED: ESH	INDIANA DEPARTMENT OF TRANSPORTATION PLAN & PROFILE LINE "PR-S-1-B" (STACY ROAD)	HORIZONTAL SCALE 1" = 20' VERTICAL SCALE 1" = 5' SURVEY BOOK N/A CONTRACT N/A	BRIDGE FILE N/A DESIGNATION 1802805 SHEETS 9 OF 28 PROJECT 1802805
--	--	---	---	--	---



Begin Project
Sta. 10+20.00 Line "PR-B"
= Sta. 139+37.79 Line "B", 0.45' Lt.

TEMPORARY EROSION CONTROL LEGEND

-  Check Dam, Revetment-Riprap
-  Site Fence

Note: Paved Inlet Protection (Not Shown)
Shall be Provided For All Existing &
Proposed Inlets Within The Construction Limits

-  PCCP for Approaches, 6" on Dense Graded Subbase, 6" on Subgrade Treatment, Type II
-  Truck Apron
Decorative PCCP, 7" on 8" Compacted Aggregate, No. 53, Base (15' Maximum Joint Spacing With 1" Dowel Bars)
-  220 #/Sys QC/QA-HMA, 3, 76, Surface, 12.5 mm on 275 #/Sys QC/QA-HMA, 3, 76, Intermediate, 19.0 mm on 385 #/Sys QC/QA-HMA, 3, 64, Base, 19.0 mm on 330 #/Sys QC/QA-HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment, 1B

CONSTRUCTION DETAILS LEGEND

-  Curb & Gutter, Concrete, Modified
-  Curb & Gutter, B, Concrete, Modified
-  Concrete Curb, Modified
-  Curb, Integrated, Concrete
-  Center Curb, D, Concrete
-  Nursery Sodding
-  Seed Mixture, U

-  Proposed Ditch Flow Line
-  Existing Ditch Flow Line

Note: See Survey Control Sheet For Survey Lines "B" & "S-1-B" Information. See Plan & Profile Sheets For Alignments "PR-B" & "PR-S-1-B" Information. See Geometric Details Sheet For Additional Alignments Details. Line "PR-B" To Be Built.



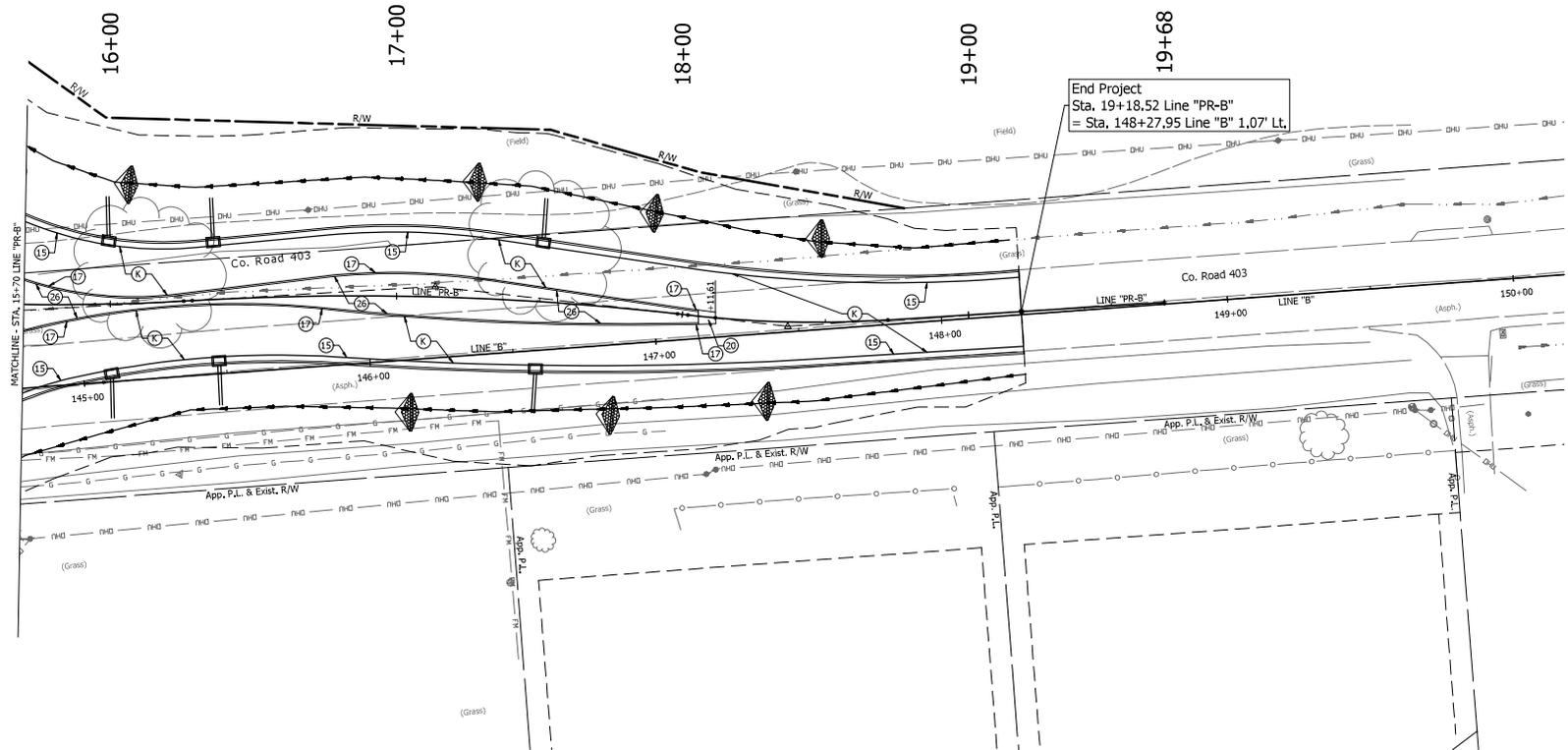
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Indianapolis, IN 46250
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ESH	DRAWN: VAD	
CHECKED: JEL	CHECKED: ESH	

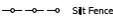
INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-B" (COUNTY ROAD 403)

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1802805
SURVEY BOOK	SHEETS
N/A	10 of 28
CONTRACT	PROJECT
N/A	1802805



TEMPORARY EROSION CONTROL LEGEND

-  Check Dam, Revetment-Riprap
-  Silt Fence

Note: Paved Inlet Protection (Not Shown) Shall Be Provided For All Existing & Proposed Inlets Within The Construction Limits

CONSTRUCTION DETAILS LEGEND

-  15 Curb & Gutter, Concrete, Modified
-  16 Curb & Gutter, B, Concrete, Modified
-  17 Concrete Curb, Modified
-  18 Curb, Integrated, Concrete
-  20 Center Curb, D, Concrete
-  26 Nursery Sodding
-  27 Seed Mixture, U

-  Proposed Ditch Flow Line
-  Existing Ditch Flow Line

Note: See Survey Control Sheet For Survey Lines "B" & "S-1-B" Information. See Plan & Profile Sheets For Alignments "PR-B" & "PR-S-1-B" Information. See Geometric Details Sheet For Additional Alignments Details. Line "PR-B" To Be Built.



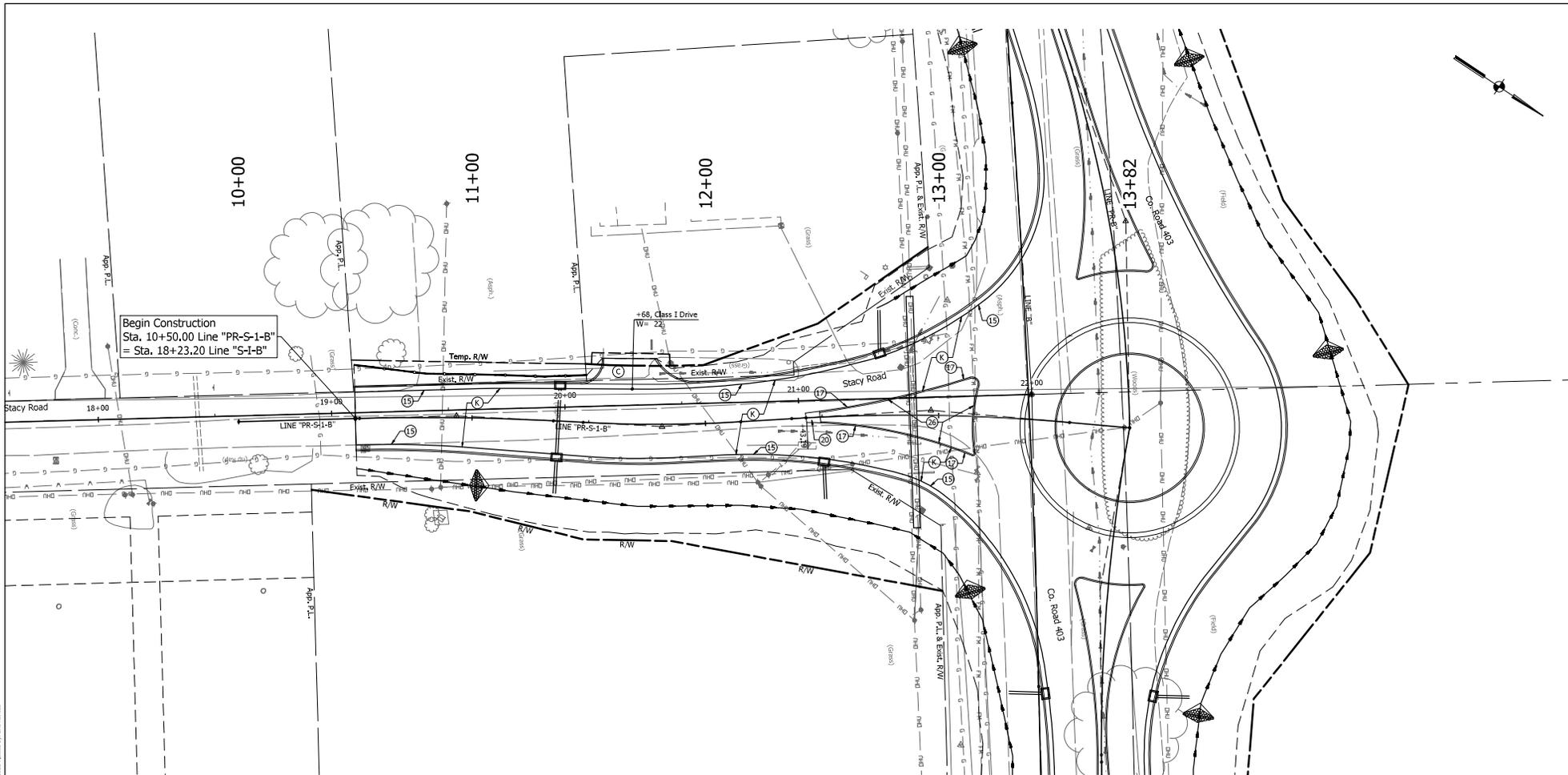
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DESIGNED: ESH	DRAWN: VAD	
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INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-B" (COUNTY ROAD 403)

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1802805
SURVEY BOOK	SHEETS
N/A	11 of 28
CONTRACT	PROJECT
N/A	1802805



Begin Construction
Sta. 10+50.00 Line "PR-S-1-B"
= Sta. 18+23.20 Line "S-1-B"

TEMPORARY EROSION CONTROL LEGEND

- Check Dam, Revetment-Riprap
- Silt Fence

Note: Paved Inlet Protection (Not Shown)
Shall Be Provided For All Existing &
Proposed Inlets Within The Construction Limits

- PCCP for Approaches, 6" on Dense Graded Subbase, 6" on Subgrade Treatment, Type II
- Truck Apron
Decorative PCCP, 7" on 8" Compacted Aggregate, No. 53, Base (15" Maximum Joint Spacing With 1" Dowel Bars)
- 220 #/Sys QC/QA+HMA, 3, 76, Surface, 12.5 mm on 275 #/Sys QC/QA+HMA, 3, 76, Intermediate, 19.0 mm on 385 #/Sys QC/QA+HMA, 3, 64, Base, 19.0 mm on 330 #/Sys QC/QA+HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment, 1B

CONSTRUCTION DETAILS LEGEND

- 15 Curb & Gutter, Concrete, Modified
- 16 Curb & Gutter, B, Concrete, Modified
- 17 Concrete Curb, Modified
- 18 Curb, Integrated, Concrete
- 20 Center Curb, D, Concrete
- 26 Nursery Sodding
- 27 Seed Mixture, U
- Proposed Ditch Flow Line
- Existing Ditch Flow Line

Note: See Survey Control Sheet For Survey Lines "B" & "S-1-B" Information. See Plan & Profile Sheets For Alignments "PR-B" & "PR-S-1-B" Information. See Geometric Details Sheet For Additional Alignments Details. Line "PR-S-1-B" To Be Built.



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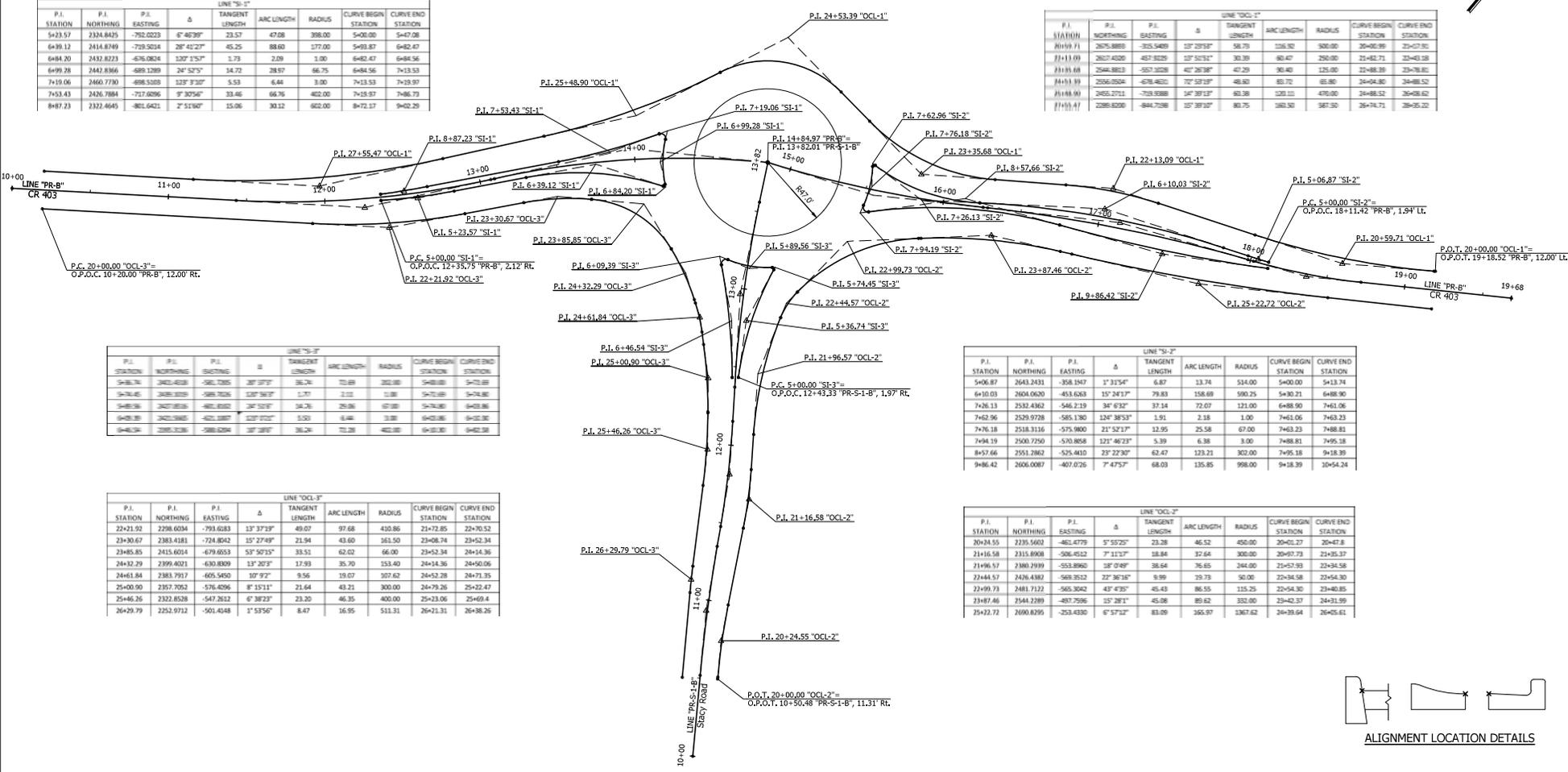
CONSTRUCTION DETAILS
LINE "PR-S-1-B" (STACY ROAD)

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1802805
SURVEY BOOK	SHEETS
N/A	12 21 28
CONTRACT	PROJECT
N/A	1802805



LINE "SI-1"									
P.I. STATION	P.I. NORTHING	P.I. EASTING	Δ	TANGENT LENGTH	ARC LENGTH	RADIUS	CURVE BEGIN STATION	CURVE END STATION	
5+23.57	2324.8425	-752.0233	6° 46'38"	23.57	47.08	398.00	5+00.00	5+47.08	
6+39.12	2414.8749	-729.5034	28° 45'27"	45.25	88.60	177.00	5+93.87	6+42.47	
6+84.20	2432.8223	-676.0824	130° 1'57"	1.73	2.09	1.00	6+82.47	6+84.56	
6+99.28	2442.8366	-689.1289	24° 52'57"	14.72	28.97	66.75	6+84.56	7+13.53	
7+19.06	2460.7730	-698.5359	123° 3'20"	5.53	6.44	3.00	7+13.53	7+19.57	
7+53.43	2426.7884	-717.6096	9° 30'56"	33.46	66.76	402.00	7+19.57	7+86.73	
8+87.23	2332.8445	-861.9423	2° 51'56"	15.06	30.12	602.00	8+72.17	9+02.29	

LINE "OCL-1"									
P.I. STATION	P.I. NORTHING	P.I. EASTING	Δ	TANGENT LENGTH	ARC LENGTH	RADIUS	CURVE BEGIN STATION	CURVE END STATION	
10+19.71	2675.8869	-325.5469	137° 23'58"	58.79	126.92	500.00	20+00.99	21+07.91	
11+11.09	2627.4330	-457.8329	123° 52'54"	33.39	60.47	250.00	21+43.71	22+43.39	
11+35.68	2546.8813	-557.3028	42° 28'36"	47.29	90.40	125.00	22+88.39	23+78.81	
11+51.89	2556.0504	-478.4632	70° 53'23"	46.80	85.72	65.00	24+04.80	24+86.52	
11+64.90	2455.2711	-729.9388	147° 53'23"	63.38	133.11	475.00	24+86.52	26+48.62	
11+91.47	2385.6300	-894.7038	157° 30'20"	83.75	163.50	587.50	26+74.71	28+25.22	

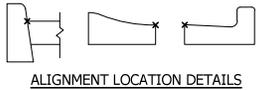


LINE "SI-2"									
P.I. STATION	P.I. NORTHING	P.I. EASTING	Δ	TANGENT LENGTH	ARC LENGTH	RADIUS	CURVE BEGIN STATION	CURVE END STATION	
5+06.87	2643.2431	-358.1547	1° 31'54"	6.87	13.74	514.00	5+00.00	5+13.74	
6+10.03	2604.0620	-453.6263	15° 24'17"	79.83	158.69	590.25	5+30.21	6+88.90	
7+26.13	2532.4362	-546.7219	34° 6'32"	37.14	72.07	121.00	6+88.90	7+61.06	
7+62.96	2529.9728	-585.1302	124° 38'53"	1.91	2.18	1.00	7+61.06	7+63.23	
7+76.18	2518.3116	-575.9002	21° 52'17"	12.95	25.56	67.00	7+63.23	7+88.81	
7+94.19	2500.7250	-570.8058	121° 46'23"	5.39	6.38	3.00	7+88.81	7+95.18	
8+57.66	2551.2862	-525.4010	23° 22'30"	62.47	123.21	302.00	7+95.18	9+18.39	
9+86.42	2306.0087	-407.0725	7° 47'57"	68.03	135.85	598.00	9+18.39	10+54.24	

LINE "SI-3"									
P.I. STATION	P.I. NORTHING	P.I. EASTING	Δ	TANGENT LENGTH	ARC LENGTH	RADIUS	CURVE BEGIN STATION	CURVE END STATION	
22+21.92	2298.6034	-793.6383	137° 37'23"	40.67	97.68	439.86	21+72.85	22+76.52	
23+30.67	2383.4181	-724.8042	15° 27'49"	21.94	43.60	363.50	23+08.74	23+52.34	
23+85.85	2415.6014	-679.6553	53° 50'15"	33.51	62.02	66.00	23+52.34	24+14.36	
24+32.29	2399.4021	-630.8309	13° 20'3"	17.83	35.70	153.40	24+34.36	24+50.06	
24+61.84	2383.7917	-605.5450	10° 27"	9.56	19.07	107.62	24+52.28	24+71.35	
25+00.90	2387.7052	-576.4096	8° 15'11"	21.64	43.21	300.00	24+79.26	25+22.47	
25+46.26	2322.8528	-547.2632	6° 38'23"	23.20	46.35	400.00	25+23.06	25+69.4	
26+29.79	2252.9712	-501.4348	1° 53'56"	8.47	16.95	511.31	26+21.31	26+38.26	

LINE "OCL-3"									
P.I. STATION	P.I. NORTHING	P.I. EASTING	Δ	TANGENT LENGTH	ARC LENGTH	RADIUS	CURVE BEGIN STATION	CURVE END STATION	
5+06.87	2643.2431	-358.1547	1° 31'54"	6.87	13.74	514.00	5+00.00	5+13.74	
6+10.03	2604.0620	-453.6263	15° 24'17"	79.83	158.69	590.25	5+30.21	6+88.90	
7+26.13	2532.4362	-546.7219	34° 6'32"	37.14	72.07	121.00	6+88.90	7+61.06	
7+62.96	2529.9728	-585.1302	124° 38'53"	1.91	2.18	1.00	7+61.06	7+63.23	
7+76.18	2518.3116	-575.9002	21° 52'17"	12.95	25.56	67.00	7+63.23	7+88.81	
7+94.19	2500.7250	-570.8058	121° 46'23"	5.39	6.38	3.00	7+88.81	7+95.18	
8+57.66	2551.2862	-525.4010	23° 22'30"	62.47	123.21	302.00	7+95.18	9+18.39	
9+86.42	2306.0087	-407.0725	7° 47'57"	68.03	135.85	598.00	9+18.39	10+54.24	

LINE "OCL-2"									
P.I. STATION	P.I. NORTHING	P.I. EASTING	Δ	TANGENT LENGTH	ARC LENGTH	RADIUS	CURVE BEGIN STATION	CURVE END STATION	
20+24.55	2235.9603	-462.4779	5° 55'25"	23.28	45.52	450.00	20+02.27	20+47.8	
21+16.18	2315.8908	-526.4532	7° 11'37"	18.84	37.64	300.00	20+67.73	21+25.37	
21+46.57	2380.2999	-553.8960	38° 0'48"	38.54	76.65	244.00	21+57.93	22+34.58	
22+44.57	2426.4382	-568.3512	22° 36'36"	9.99	19.73	92.00	22+34.58	22+54.30	
22+99.73	2481.7122	-565.3042	43° 47'30"	45.43	86.55	115.25	22+54.30	23+48.30	
23+87.46	2544.2289	-487.7596	15° 28'11"	45.08	89.62	332.00	23+42.37	24+31.99	
25+22.72	2690.8295	-253.4330	6° 57'12"	83.09	166.97	1367.62	24+39.64	26+45.61	



ALIGNMENT LOCATION DETAILS



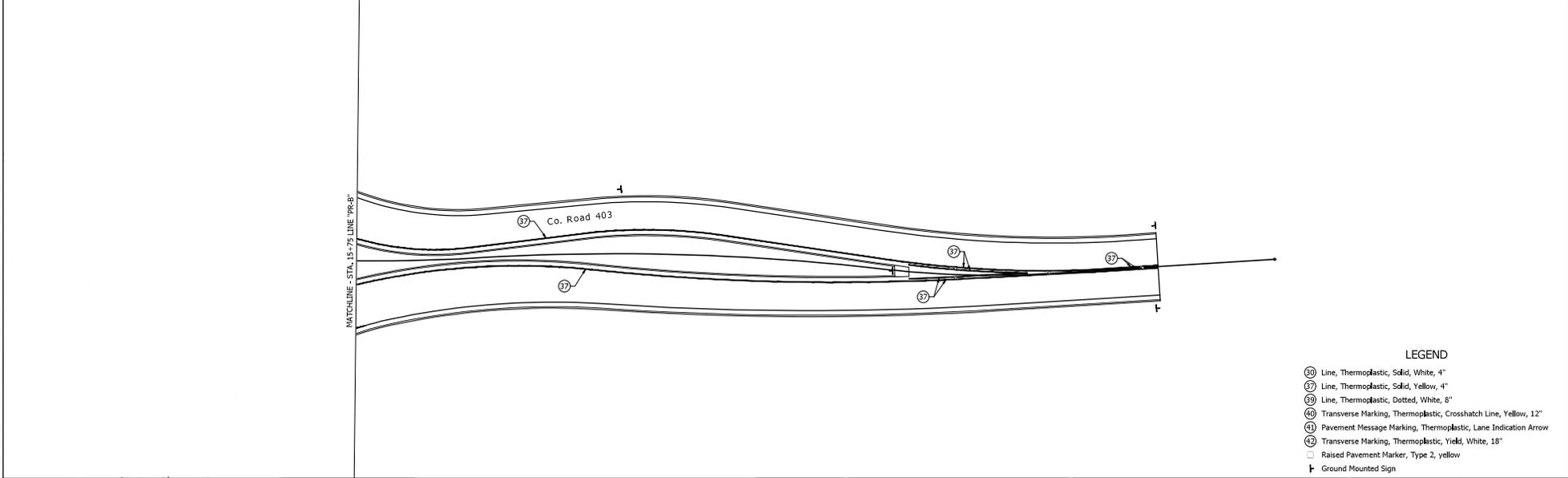
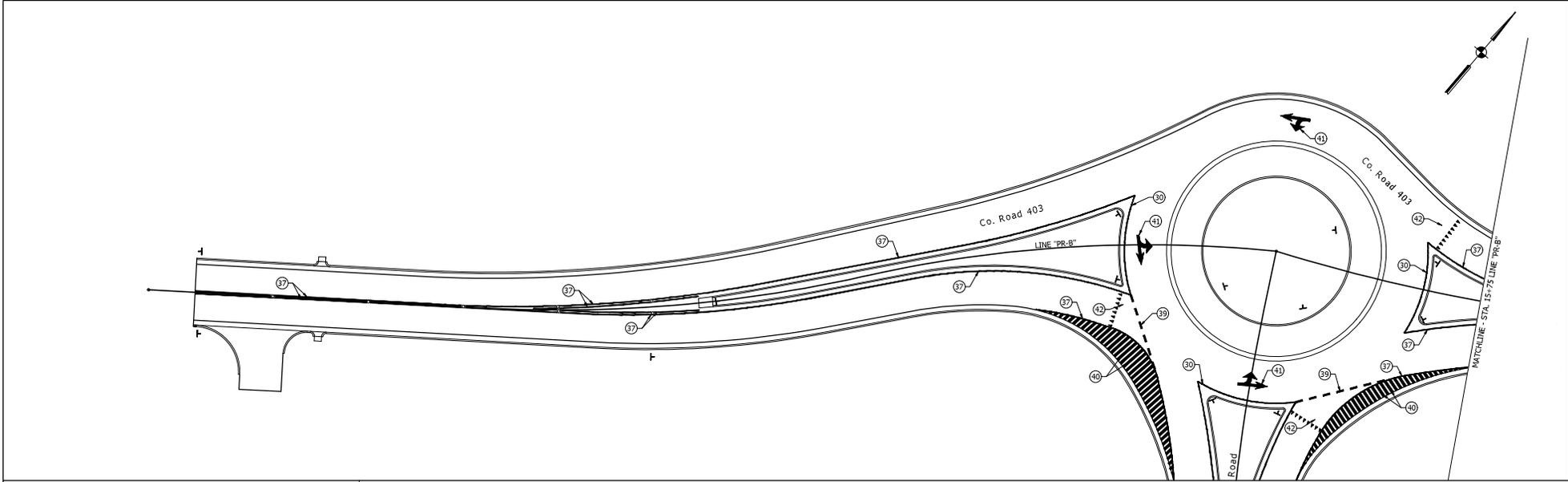
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CHECKED: JEL	CHECKED: ESH	

INDIANA
DEPARTMENT OF TRANSPORTATION

ROUNDABOUT
GEOMETRIC DETAILS

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
1" = 30'	1802805
SURVEY BOOK	SHEETS
N/A	13 OF 28
CONTRACT	PROJECT
N/A	1802805



- LEGEND**
- ③⑩ Line, Thermoplastic, Solid, White, 4"
 - ③⑦ Line, Thermoplastic, Solid, Yellow, 4"
 - ③⑧ Line, Thermoplastic, Dotted, White, 8"
 - ④⑥ Transverse Marking, Thermoplastic, Crosshatch Line, Yellow, 12"
 - ④① Pavement Message Marking, Thermoplastic, Lane Indication Arrow
 - ④② Transverse Marking, Thermoplastic, Yield, White, 18"
 - Raised Pavement Marker, Type 2, yellow
 - ↑ Ground Mounted Sign

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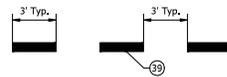
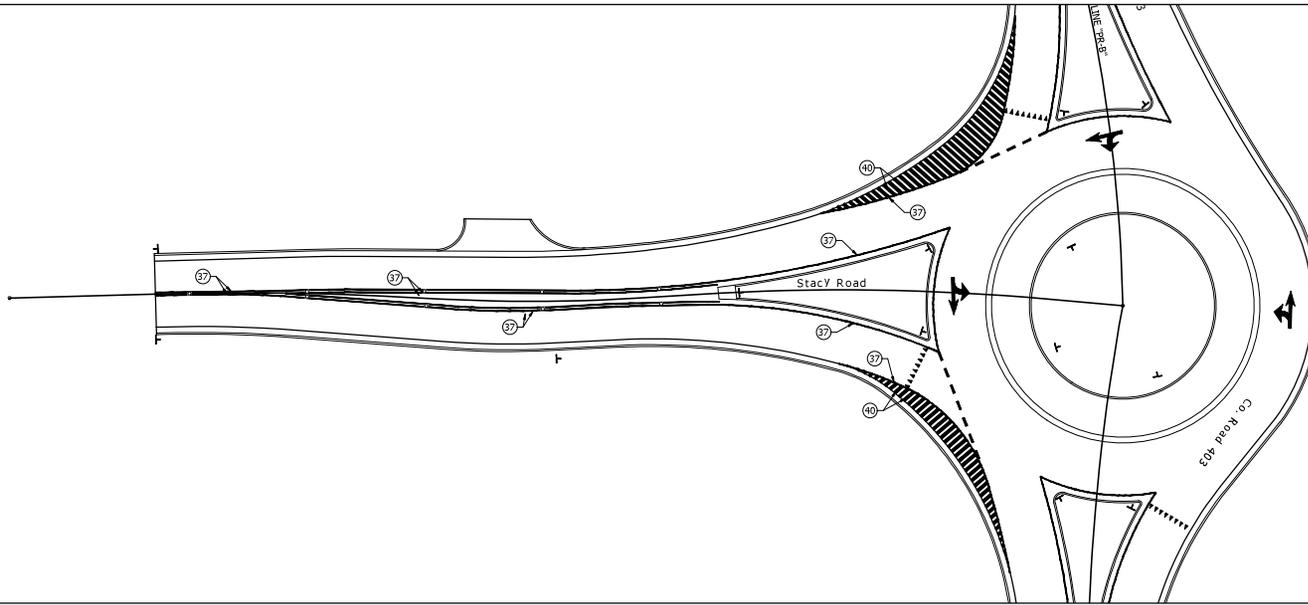


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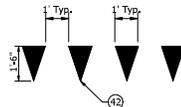
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ESH	DRAWN: VAD	
CHECKED: JEL	CHECKED: ESH	

INDIANA
 DEPARTMENT OF TRANSPORTATION
 SIGNING & PAVEMENT MARKING DETAILS
 LINE "PR-B" (COUNTY ROAD 403)

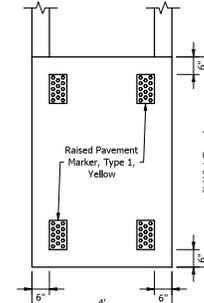
HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1802805
SURVEY BOOK	SHEETS
N/A	14 28
CONTRACT	PROJECT
N/A	1802805



DOTTED LINE DETAIL
NOT TO SCALE



YIELD LINE DETAIL
NOT TO SCALE



SPLITTER ISLAND END TREATMENT DETAIL
NOT TO SCALE

- LEGEND**
- ③⑦ Line, Thermoplastic, Solid, White, 4"
 - ③⑦ Line, Thermoplastic, Solid, Yellow, 4"
 - ③⑨ Line, Thermoplastic, Dotted, White, 8"
 - ④① Transverse Marking, Thermoplastic, Crosshatch Line, Yellow, 12"
 - ④① Pavement Message Marking, Thermoplastic, Lane Indication Arrow
 - ④② Transverse Marking, Thermoplastic, Yield, White, 18"
 - ▣ Raised Pavement Marker, Type 2, yellow
 - ▬ Ground Mounted Sign

File Name: P:\PROJECTS\1802805 - Stacy Rd Roundabout\plan\stacyroad\signing & pavement\stacyroad_signing_pavement.dwg Date: 11/17/2011 10:58:41 AM User: Eric Helmer



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CHECKED: JEL	CHECKED: ESH	

INDIANA
DEPARTMENT OF TRANSPORTATION

SIGNING & PAVEMENT MARKING DETAILS
LINE "PR-S-1-B" (STACY ROAD)

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1802805
SURVEY BOOK	SHEETS
N/A	15 OF 28
CONTRACT	PROJECT
N/A	1802805

Appendix C

Early Coordination



ENGINEERING
ENVIRONMENTAL
INSPECTION
LAND SURVEYING
LAND ACQUISITION
PLANNING
WATER &
WASTEWATER
SINCE 1965

OFFICERS

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Dave Richter, PE, PLS
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B. Keith Bryant, PE
Michael Rowe, PE

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Kelton S. Cunningham, PE
Braun S. Rodgers, PE
Chris J. Andrzejewski, PE
Greg J. Broz, PE
Joshua D. Gonya, PE
Brian S. Haefliger, PE
Ian A.R. Scott, PE
Rob B. Iversen, PE
Jeffrey E. Lazzell, PE
John SanGiorgio, PE

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8440 Allison Pointe Blvd., Suite 200, Indianapolis, IN 46250

April 20, 2020

Ms. Erica Tait
Federal Highway Administration
Indiana Division
575 N. Pennsylvania Street
Indianapolis, Indiana 46204

RE: Early Coordination Letter
County Road 403 at Stacy Road
Intersection Improvement Project
Clark County, Indiana
Des. No: 1802805

Dear Ms. Tait,

Officials with Clark County intend to proceed with a project at the intersection of County Road 403 and Stacy Road. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental impacts. This project is partially federally funded (Federal Highway Administration) with an 80% federal and 20% local split.

Our firm has been retained by Clark County to undertake the required preliminary engineering activities for this project. Please refer to the attached location maps and ground level photographs to assist with your review.

This anticipated roundabout project is located at the intersection of County Road 403 and Stacy Road, approximately 2.3 miles north of US 31, in Clark County. County Road 403 is a two lane Other-Principal Arterial and Stacy Road is a local two lane road. County Road 403 currently consists of two 12-foot travel lanes, and Stacy Road is composed of two 10-foot travel lanes both with bituminous wearing surfaces. No pedestrian facilities exist within the project limits.

Land use in the vicinity of the project is primarily residential. Our firm will perform waters and wetlands determinations to identify any potential jurisdictional water resources that may be present. This project qualifies for the application of the USFWS range-wide programmatic informal consultation for the Indiana bat and northern long eared bat and project information will be submitted through USFWS's Information for Planning and Consultation (IPaC) separately. Our firm will investigate the areas of additional right-of-way for archaeological and historic resources for Section 106 compliance. The results of this investigation will be forwarded to the State Historic Preservation Officer (SHPO) for review and concurrence.

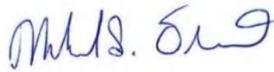
Should we not receive your response within thirty (30) calendar days from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request. Please address your agency's response to Michael S. Oliphant, Environmental Specialist, United Consulting, 8440 Allison Pointe Blvd., Suite 200, Indianapolis, Indiana

Early Coordination Letter
County Road 403 at Stacy Road
Intersection Improvement Project
Des. No. 1802805
Page 2 of 2

46250, (317) 895-2585, email mike.oliphant@ucindy.com or Brian Dixon, Clark County Highway Engineer, 501 East Court Avenue, Room 404, Jeffersonville, IN 47130, (812) 285-6276, email bdixon@co.clark.in.us.

Your cooperation in expediting the development of this project is appreciated. Officials with Clark County want to advance this project as soon as possible to provide for a safe and efficient facility. If you need additional information or have any questions, please contact our office.

Sincerely,
UNITED CONSULTING



Michael S. Oliphant
Environmental Specialist

GRAPHICS HAVE BEEN INCLUDED IN APPENDIX B

enclosures: Location Maps
Ground Level Photographs

c: Natural Resources Conservation Service
Indiana Department of Environmental Management
US Fish and Wildlife Service
US Army Corps of Engineers
Federal Highway Administration
Indiana Department of Natural Resources – Division of Fish & Wildlife
INDOT Aeronautics Division
INDOT Seymour District
INDOT Environmental Services
INDOT Public Involvement
HUD
Indiana Geological Survey
National Park Service
KIPDA
Pleasant Grove Methodist Church
Clark County Highway Department
Clark County Board of Commissioners
Aggrock Quarry
UNITED File: (19-408)

LIST OF EARLY COORDINATION RECIPIENTS

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-22467

Request Received: April 21, 2020

Requestor: United Consulting
Michael S Oliphant
8440 Allison Pointe Boulevard, Suite 200
Indianapolis, IN 46250

Project: CR 403 and Stacy Road intersection roundabout construction, about 2.3 miles north of US 31; Des #1802805

County/Site info: Clark

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: The Division of Fish & Wildlife recommends avoiding impacts to the forested drainage west of the intersection, if possible. We recommend a mitigation plan be developed for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at:
<http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in and urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites however.

We recommend planting a mixture of native grasses, wildflowers, and also native hardwood trees and shrubs in any splitter or center island landscaping.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of native grasses, sedges, wildflowers, and also native hardwood trees and shrubs if any woody plants are disturbed during construction as soon as possible upon completion. Do not use any

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

varieties of Tall Fescue or other non-native plants, including prohibited invasive species (see 312 IAC 18-3-25).

2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.

3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.

4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

5. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife

Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.

Christie L. Stanifer

Date: May 20, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

May 7, 2020

Michael S. Oliphant
United Consulting
8440 Allison Pointe Boulevard, Suite 200
Indianapolis, Indiana 46250

Dear Mr. Oliphant:

The proposed project to proceed with intersection improvements along County Road 403 at Stacy Road in Clark County, Indiana, (Des No 1802805) as referred to in your letter received April 20, 2020, will cause a conversion of prime farmland.

The attached packet of information is for your use competing Parts VI and VII of the AD-1006. After completion, the federal funding agency needs to forward one copy to NRCS for our records.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICK NEILSON
State Soil Scientist

Enclosures

Helping People Help the Land.



USDA is an equal opportunity provider, employer and lender.

FARMLAND CONVERSION IMPACT RATING

PART I (To be completed by Federal Agency)		Date Of Land Evaluation Request			
Name of Project DES1802805 CR403 StacyRd		Federal Agency Involved FHWA			
Proposed Land Use		County and State Clark County, Indiana			
PART II (To be completed by NRCS)		Date Request Received By NRCS 4/20/2020		Person Completing Form: JRA	
Does the site contain Prime, Unique, Statewide or Local Important Farmland? <i>(If no, the FPPA does not apply - do not complete additional parts of this form)</i>		YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>		Acres Irrigated	
				Average Farm Size 195 ac	
Major Crop(s) Corn		Farmable Land In Govt. Jurisdiction Acres: 165994 % 69		Amount of Farmland As Defined in FPPA Acres: 106482 % 44	
Name of Land Evaluation System Used LESA		Name of State or Local Site Assessment System		Date Land Evaluation Returned by NRCS 5/7/2020	
PART III (To be completed by Federal Agency)		Alternative Site Rating			
		Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly		0.62			
B. Total Acres To Be Converted Indirectly		0.00			
C. Total Acres In Site		0.62			
PART IV (To be completed by NRCS) Land Evaluation Information					
A. Total Acres Prime And Unique Farmland		0.04			
B. Total Acres Statewide Important or Local Important Farmland		0.00			
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted		<0.001			
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value		52			
PART V (To be completed by NRCS) Land Evaluation Criterion Relative Value of Farmland To Be Converted (Scale of 0 to 100 Points)		82			
PART VI (To be completed by Federal Agency) Site Assessment Criteria <i>(Criteria are explained in 7 CFR 658.5 b. For Corridor project use form NRCS-CPA-106)</i>		Maximum Points	Site A	Site B	Site C
1. Area In Non-urban Use		(15)	15		
2. Perimeter In Non-urban Use		(10)	0		
3. Percent Of Site Being Farmed		(20)	10		
4. Protection Provided By State and Local Government		(20)	0		
5. Distance From Urban Built-up Area		(15)	5		
6. Distance To Urban Support Services		(15)	5		
7. Size Of Present Farm Unit Compared To Average		(10)	0		
8. Creation Of Non-farmable Farmland		(10)	10		
9. Availability Of Farm Support Services		(5)	0		
10. On-Farm Investments		(20)	0		
11. Effects Of Conversion On Farm Support Services		(10)	0		
12. Compatibility With Existing Agricultural Use		(10)	10		
TOTAL SITE ASSESSMENT POINTS		160	55	0	0
PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)		100	82	0	0
Total Site Assessment (From Part VI above or local site assessment)		160	55	0	0
TOTAL POINTS (Total of above 2 lines)		260	137	0	0
Site Selected:		Date Of Selection		Was A Local Site Assessment Used?	
				YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>	
Reason For Selection:					
Name of Federal agency representative completing this form: Micheal S. Oliphant				Date: May 8, 2020	

(See Instructions on reverse side)

Form AD-1006 (03-02)

Organization and Project Information

Project ID: 19-408
Des. ID: 1802805
Project Title: County Road 403 at Stacy Road Intersection Improvement Project
Name of Organization: United Consulting
Requested by: Michael Oliphant

Environmental Assessment Report

1. Geological Hazards:
 - Potential Karst
 - Moderate liquefaction potential
 - 1% Annual Chance Flood Hazard
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: None documented in the area
3. Active or abandoned mineral resources extraction sites:
 - Petroleum Exploration Wells

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

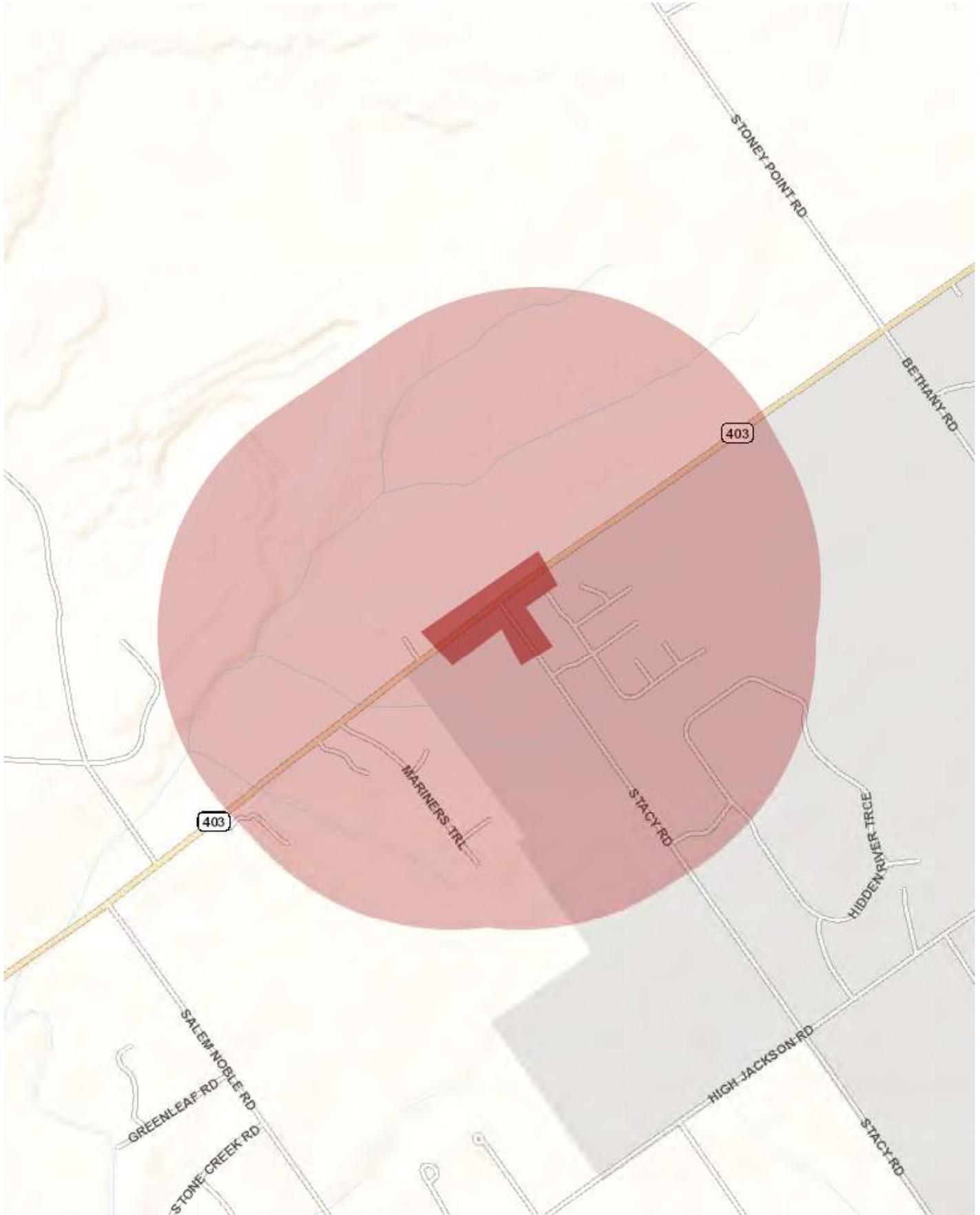
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: April 21, 2020



Metadata:

- https://maps.indiana.edu/metadata/Hydrology/Karst_Cave_Density.html
- https://maps.indiana.edu/metadata/Hydrology/Karst_Sinkhole_Areas.html
- https://maps.indiana.edu/metadata/Geology/Petroleum_Wells.html
- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204

(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

Clark County Highway Department
Brian Dixon, Highway Engineer
501 East Court Avenue, Room 404
Jeffersonville, IN 47130

United Consulting
Aaron M. Toombs, Environmental Specialist
8440 Allison Pointe Boulevard, Suite 200
Indianapolis, IN 46250

Date: January 19, 2021

To Engineers and Consultants Proposing Roadway Construction Projects:

The proposed project will involve construction of a single lane roundabout at the existing County Road (CR) 403 and Stacy Road intersection. The center of the roundabout will be shifted to the north to avoid the church parcel and utility conflicts. The inscribed diameter will be 130.0 feet and include an 18.0 foot circulatory lane width with an 8.0 foot truck apron. The roundabout and approaches were designed to handle a WB-50 truck. The intersection improvement will slow vehicles on CR 403 and provide turning movements to and from Stacy Road.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm>.

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices

(<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm>. IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm>.

If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana . A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.

If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek

additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> for the appropriate staff contact to further discuss your project. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:

IC 14-26-2 Lakes Preservation Act 312 IAC 11
IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
IC 14-28-1 Flood Control Act 310 IAC 6-1
IC 14-29-1 Navigable Waterways Act 312 IAC 6
IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm>. Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page <http://www.in.gov/idem/4902.htm>

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF], pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>). Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation. Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm>.

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for additional project input.

For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.

For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.

For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations.

Consideration should be given to the following:

Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated

in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm>.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>, <http://www.in.gov/idem/4145.htm>, or <http://www.epa.gov/radon/index.html>.

With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>.

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will

be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm>.

With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm>.

Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).

If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf.) New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.

For more information on air permits visit: <http://www.in.gov/idem/4223.htm>, or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD at dem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm>.

If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.

If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).

If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm>.

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that is it the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm>, is used.

Signature(s) of the Applicant:

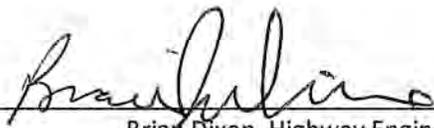
I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

The proposed project will involve construction of a single lane roundabout at the existing County Road (CR) 403 and Stacy Road intersection. The center of the roundabout will be shifted to the north to avoid the church parcel and utility conflicts. The inscribed diameter will be 130.0 feet and include an 18.0 foot circulatory lane width with an 8.0 foot truck apron. The roundabout and approaches were designed to handle a WB-50 truck. The intersection improvement will slow vehicles on CR 403 and provide turning movements to and from Stacy Road.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 1-24-21
Signature of the INDOT
Project Engineer or Other Responsible Agent


Brian Dixon, Highway Engineer

Date: January 19, 2021
Signature of the For Hire Consultant:


Aaron Toombs, Environmental Specialist

From: [Courtade, Julian](#)
To: [Mike Campbell](#)
Subject: RE: Des. No: 1802805 (County Road 403 at Stacy Road Intersection Improvement Project)
Date: Tuesday, April 21, 2020 11:04:05 PM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)

Mike –

I reviewed the Early Coordination Letter and found no issues with surrounding airspace or airports. This is due to the project meeting the required glideslope requirements to the nearest public-use facility. Please let me know if you have any questions!

Thanks,

Julian L. Courtade

Chief Airport Inspector

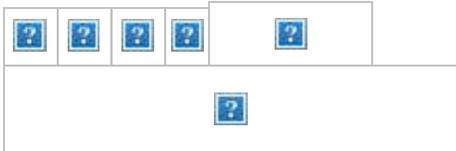
100 North Senate Ave, N955

Indianapolis, IN 46204

Office: (317) 232-1477

Cell: (317) 954-7385

Email: jcourtade@indot.in.gov



From: Mike Campbell <Mike.Campbell@ucindy.com>
Sent: Tuesday, April 21, 2020 2:15 PM
To: Courtade, Julian <JCourtade@indot.IN.gov>
Cc: Mike Oliphant <Mike.Oliphant@ucindy.com>
Subject: Des. No: 1802805 (County Road 403 at Stacy Road Intersection Improvement Project)

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Dear Mr. Courtade,

Please find the attached Early Coordination Letter for your review. If you have any questions, comments, or need additional information, please do not hesitate to contact me.

Regards,

From: [McWilliams, Robin](#)
To: [Mike Campbell](#)
Subject: Re: [EXTERNAL] Des. No: 1802805 (County Road 403 at Stacy Road Intersection Improvement Project)
Date: Tuesday, April 28, 2020 2:31:58 PM
Attachments: [image001.png](#)

Dear Mike,

This responds to your recent letter requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (i.e. a federal transportation nexus is established). The Service has 14 days after a "Not Likely to Adversely Affect" determination letter is generated to review the project and provide additional comments or request additional information; if you do not receive a response from us within 14 days, we have no additional comments.

The project is in the karst area of Indiana. If any karst features are encountered, a karst survey should be conducted, with mitigation measures as necessary, in accordance with our 1993 Memorandum of Understanding.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no other comments on the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel,

cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing

Robin McWilliams Munson
Fish and Wildlife Biologist
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, IN 46142
812-334-4261

[Mon-Tues 8-3:30p](#)

[Wed-Thurs 8:30-3p Telework](#)

From: Mike Campbell <Mike.Campbell@ucindy.com>

Sent: Tuesday, April 21, 2020 2:13 PM

To: McWilliams, Robin <robin_mcwilliams@fws.gov>

Cc: Mike Oliphant <Mike.Oliphant@ucindy.com>

Subject: [EXTERNAL] Des. No: 1802805 (County Road 403 at Stacy Road Intersection Improvement Project)

Good afternoon,

Please find the attached Early Coordination Letter for your review. If you have any questions, comments, or need additional information, please do not hesitate to contact me.

Regards,

MIKE CAMPBELL
Planning / Environmental
United Consulting

From: [Dye, David](#)
To: [Aaron Toombs](#)
Subject: RE: IPaC Process for Des. No.: 1802805 - CR 403 and Stacy Road Intersection Improvement Project
Date: Friday, January 29, 2021 4:40:57 PM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)

Hi Aaron,

I have reviewed and submitted this determination to USFWS for their 14-day review period.

For the future, please do keep in mind my comments about a complete but concise description.

Let me know if you have any additional questions.

David Dye

Environmental Section Manager

185 Agrico Lane

Seymour, IN 47274

Office: (812) 524-3723

Email: ddye@indot.in.gov



From: Aaron Toombs <Aaron.Toombs@ucindy.com>
Sent: Tuesday, January 19, 2021 5:26 PM
To: Dye, David <DDYE@indot.IN.gov>
Subject: RE: IPaC Process for Des. No.: 1802805 - CR 403 and Stacy Road Intersection Improvement Project

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David,

The IPaC determination key for Des. No.: 1802805 has been revised to include the addition of permanent lighting as part of the project. I have updated the final paragraph in the project description to reflect this as well.

When you have a moment available, could you please review the updated information within IPaC and verify the determination key is completed and correct?



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

January 19, 2021

Consultation Code: 03E12000-2021-SLI-0356

Event Code: 03E12000-2021-E-02706

Project Name: Des. No.: 1802805 - County Road 403 and Stacy Road Intersection Improvement Project

Subject: Updated list of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2021-SLI-0356

Event Code: 03E12000-2021-E-02706

Project Name: Des. No.: 1802805 - County Road 403 and Stacy Road Intersection
Improvement Project

Project Type: TRANSPORTATION

Project Description: Officials with Clark County intend to proceed with an intersection improvement project at the intersection of County Road 403 and Stacy Road in Clark, County, Indiana (Des. No.: 1802805). The proposed project includes constructing a roundabout at the existing intersection which will utilize a newly designed pavement section and is expected to have an Inscribed Circle Diameter (ICD) of 134.0 feet. The approaches along CR 403 and Stacy Road will be accommodated with an 18.0 foot circulatory lane for each leg of the roundabout. The construction of the roundabout is intended to provide drivers with a safer intersection by reducing vehicular speeds and reducing accident frequency and severity. New curb turnouts will be constructed, as needed, along the proposed curb lines, with revetment riprap connecting to the existing roadside ditches. This allows the existing open ditch drainage to function as originally designed with minimal disturbance. Storm sewer design has not been included in the project scope; however, if recommended by the geotechnical report, new underdrains will be installed along the new curb lines. Additionally, including a pervious center island and pervious splitter islands are expected to reduce the overall runoff from the intersection. As a result, detention is not included in the project scope. This project will not involve any impacts to streams or wetlands. Regulatory Waterway Permitting will not be required. Two small maintenance pipe culverts (Structure 1 and Structure 2) with diameters of 15.0 and 12.0 inches, respectively, are located within the project area. The maintenance pipes act as drainage features during rainfall events and do not carry any jurisdictional features. Structure 1 could not be fully inspected for the presence or absence of bats as the inlet of Structure 1 was partially buried and the pipe was heavily silted-in throughout, resulting in very limited accessibility. It is assumed bats do not frequent Structure 1 due to limited accessibility in addition to the likelihood of any rainfall event flooding the remainder of the silted-in pipe, which would not be conducive to bat roosting.

The preferred Maintenance of Traffic (MOT) plan will involve a full road closure and use of a detour route. Eastbound traffic will be redirected south on John Noble Road and back East along High Jackson Road. Westbound traffic will be redirected south along Stoney Point Road and then back west along High Jackson Road. Pedestrian features are not present at the project location and as a result, no pedestrian MOT will be

required for this project.

It is believed that suitable summer habitat for the Indiana bat and Northern long-eared bat exists near the proposed project area. Approximately 0.28 acre will be removed. The dominant tree species to be removed are sugar maple (*Acer saccharum*) and black walnut (*Juglans nigra*). Tree removal will occur during the inactive season (between October 1 and March 29). A review of the USFWS Database by INDOT Seymour District Environmental staff on December 15, 2020 did not indicate the presence of endangered or threatened bat species in or within 0.5 mile of the project area. A Red Flag Investigation (RFI) was completed for the project and approved by INDOT Site Assessment and Management on March 15, 2020. Results of the RFI did not indicate the presence of endangered bat species in or within 0.5 mile of the project area

New permanent lighting will be required for this project. The use of temporary lighting is not expected; however temporary lighting may be used if required by the construction schedule. Any use of temporary lighting will be directed away from forested habitats and will follow the BUG criteria. New permanent lighting will also be designed to follow the BUG criteria. The project has an anticipated letting date in February 2021 with construction activities expected to occur between April 2022 and September 2022.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.42600005323355,-85.71999746232886,14z>



Counties: Clark County, Indiana

Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Gray Bat <i>Myotis grisescens</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6329	Endangered
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

January 29, 2021

Consultation code: 03E12000-2021-I-0356

Event Code: 03E12000-2021-E-03186

Project Name: Des. No.: 1802805 - County Road 403 and Stacy Road Intersection Improvement Project

Subject: Concurrence verification letter for the 'Des. No.: 1802805 - County Road 403 and Stacy Road Intersection Improvement Project' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Des. No.: 1802805 - County Road 403 and Stacy Road Intersection Improvement Project** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Gray Bat *Myotis grisescens* Endangered

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. No.: 1802805 - County Road 403 and Stacy Road Intersection Improvement Project

Description

Officials with Clark County intend to proceed with an intersection improvement project at the intersection of County Road 403 and Stacy Road in Clark, County, Indiana (Des. No.: 1802805). The proposed project includes constructing a roundabout at the existing intersection which will utilize a newly designed pavement section and is expected to have an Inscribed Circle Diameter (ICD) of 134.0 feet. The approaches along CR 403 and Stacy Road will be accommodated with an 18.0 foot circulatory lane for each leg of the roundabout. The construction of the roundabout is intended to provide drivers with a safer intersection by reducing vehicular speeds and reducing accident frequency and severity. New curb turnouts will be constructed, as needed, along the proposed curb lines, with revetment riprap connecting to the existing roadside ditches. This allows the existing open ditch drainage to function as originally designed with minimal disturbance. Storm sewer design has not been included in the project scope; however, if recommended by the geotechnical report, new underdrains will be installed along the new curb lines. Additionally, including a pervious center island and pervious splitter islands are expected to reduce the overall runoff from the intersection. As a result, detention is not included in the project scope. This project will not involve any impacts to streams or wetlands. Regulatory Waterway Permitting will not be required. Two small maintenance pipe culverts (Structure 1 and Structure 2) with diameters of 15.0 and 12.0 inches, respectively, are located within the project area. The maintenance pipes act as drainage features during rainfall events and do not carry any jurisdictional features. Structure 1 could not be fully inspected for the presence or absence of bats as the inlet of Structure 1 was partially buried and the pipe was heavily silted-in throughout, resulting in very limited accessibility. It is assumed bats do not frequent Structure 1 due to limited accessibility in addition to the likelihood of any rainfall event flooding the remainder of the silted-in pipe, which would not be conducive to bat roosting.

The preferred Maintenance of Traffic (MOT) plan will involve a full road closure and use of a detour route. Eastbound traffic will be redirected south on John Noble Road and back East along High Jackson Road. Westbound traffic will be redirected south along Stoney Point Road and then back west along High Jackson Road. Pedestrian features are not present at the project location and as a result, no pedestrian MOT will be required for this project.

It is believed that suitable summer habitat for the Indiana bat and Northern long-eared bat exists near the proposed project area. Approximately 0.28 acre will be removed. The dominant tree species to be removed are sugar maple (*Acer sarccharum*) and black walnut (*Juglans nigra*). Tree removal will occur during the inactive season (between October 1 and March 29). A review of the USFWS Database by INDOT Seymour District Environmental

staff on December 15, 2020 did not indicate the presence of endangered or threatened bat species in or within 0.5 mile of the project area. A Red Flag Investigation (RFI) was completed for the project and approved by INDOT Site Assessment and Management on March 15, 2020. Results of the RFI did not indicate the presence of endangered bat species in or within 0.5 mile of the project area

New permanent lighting will be required for this project. The use of temporary lighting is not expected; however temporary lighting may be used if required by the construction schedule. Any use of temporary lighting will be directed away from forested habitats and will follow the BUG criteria. New permanent lighting will also be designed to follow the BUG criteria. The project has an anticipated letting date in February 2021 with construction activities expected to occur between April 2022 and September 2022.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) *Federal Highway Administration (FHWA)*

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

Yes

8. Will the project include *any* type of activity that could impact a **known** hibernaculum^[1], or impact a karst feature (e.g., sinkhole, losing stream, or spring) that could result in effects to a **known** hibernaculum?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

9. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

10. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

11. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

12. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

13. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

14. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

15. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

16. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

17. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

18. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

19. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

20. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

21. Are *all* trees that are being removed clearly demarcated?
Yes
22. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?
No
23. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?
No
24. Does the project include slash pile burning?
No
25. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?
Yes
26. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

27. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Structure 2 - USFWS Structure Inspection for ETR Bat Species.pdf* <https://ecos.fws.gov/ipac/project/KHCP75JWENFKZAXKH7ZVMZ6WNY/projectDocuments/98122445>
- *Structure 1 - USFWS Structure Inspection for ETR Bat Species.pdf* <https://ecos.fws.gov/ipac/project/KHCP75JWENFKZAXKH7ZVMZ6WNY/projectDocuments/98122446>

28. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

29. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

30. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

31. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

32. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

33. Will the project install new or replace existing **permanent** lighting?

Yes

34. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting will be installed or replaced?

Yes

35. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

36. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

37. Will the project raise the road profile **above the tree canopy**?

No

38. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

39. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

40. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

41. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

42. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

43. **Hibernacula AMM 1**

Will the project ensure that on-site personnel will use best management practices^[1], secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula?

[1] Coordinate with the appropriate Service Field Office on recommended best management practices for karst in your state.

Yes

44. **Hibernacula AMM 1**

Will the project ensure that, where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography?

Yes

45. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

46. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

47. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

48. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

49. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

50. **Lighting AMM 2**

Will the **permanent** lighting be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

No

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

Yes

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.28

4. Please describe the proposed bridge work:

Proposed bridge work includes the replacement of two small concrete drainage culverts within the project area. The culverts do not carry any jurisdictional aquatic features.

5. Please state the timing of all proposed bridge work:

April 2022 and September 2022

6. Please enter the date of the bridge assessment:

December 23, 2020

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

HIBERNACULA AMM 1

For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300 foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 29, 2020. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

From: [Kang, Li](#)
To: [Aaron Toombs](#)
Cc: [Bowman, Sandra A](#); [Mcgill, Justus](#)
Subject: RE: Des. No.: 1802805 - CR 403 over Stacy Road (EWPO Coordination for Potential Sinkholes)
Date: Tuesday, November 10, 2020 9:30:07 AM
Attachments: [image001.png](#)

Hi Aaron,

Both Justus and I lean towards not completing a karst report. The area does seem to be heavy urbanized already. Given that this will be a new roundabout we don't think that they will be digging very deep. However the project shall follow the Karst MOU during the construction season. If you have any questions please let us know.

Thanks,

Li Kang

Ecology & Waterway Permitting Office

INDOT N. 642

cell: 317-694-7134

phone: 317-232-6766

From: Aaron Toombs <Aaron.Toombs@ucindy.com>
Sent: Monday, November 9, 2020 4:21 PM
To: Kang, Li <LKANG@indot.IN.gov>; McGill, Justus <JMcgill@indot.IN.gov>
Cc: Bowman, Sandra A <SBowman@indot.IN.gov>
Subject: RE: Des. No.: 1802805 - CR 403 over Stacy Road (EWPO Coordination for Potential Sinkholes)

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Hi Li,

Please find general location maps with the proposed Right-of-Way boundaries shown. Please let me know if you need anything else.

Thanks!

Sincerely,



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 N. Senate Avenue • Indianapolis, IN 46204

(800) 451-6027 • (317) 232-8603 • www.idem.IN.gov

Eric J. Holcomb
Governor

Bruno Pigott
Commissioner

February 5, 2021

66-33
United Consulting
Attention: Michael S. Oliphant
8440 Allison Pointe Boulevard, Suite 200
Indianapolis, Indiana 46250

Dear Michael S. Oliphant,

RE: Wellhead Protection Area
Proximity Determination
Des No 1802805
County Road 403 at Stacy Road
Intersection Improvement Project
Clark County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project area **is not located within** a Wellhead Protection Area. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases we use a 3,000 foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The project area **is not located within** a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water.

Note: the Drinking Water Branch has a self service feature which allows one to determine wellhead proximity without submitting the application form. Use the following instructions:

1. Go to <https://www.in.gov/idem/cleanwater/pages/wellhead/>
2. Use the search tool located in the upper left hand corner of the application to zoom to your site of interest by way of city, county, or address; or use the mouse to click on the site of interest displayed on the map.
3. Once the site of interest has been located and selected, use the print tool to create a .pdf of a wellhead protection area proximity determination response.

In the future please consider using this self service feature if it suits your needs.

If you have any additional questions please feel free to contact me at the address above or at (317) 233-9158 and aturnbow@idem.in.gov.

Sincerely,

Alisha Turnbow,
Environmental Manager
Ground Water Section
Drinking Water Branch
Office of Water Quality



Early Coordination

Please Reduce, Reuse, Recycle



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 Michael S. Oliphant, AICP
 E. Rachelle Pemberton, PE
 Timothy J. Coomes, PLS
 Jon E. Clodfelter, PE
 Steven R. Passey, PE
 Brian J. Pierson, PE
 Christopher L. Hammond, PE
 Paul D. Glotzbach, PE
 Brian S. Frederick, PE
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 Christopher J. Dyer, PE
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 Caleb C. Ross, PE
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 Scott G. Minnich, PE
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 Kelton S. Cunningham, PE
 Braun S. Rodgers, PE
 Chris J. Andrzejewski, PE
 Greg J. Broz, PE
 Joshua D. Gonya, PE
 Brian S. Haefliger, PE
 Ian A.R. Scott, PE
 Amanda Stevens, PE
 Rob B. Iversen, PE
 Jeffrey E. Lazzell, PE

www.ucindy.com
 (317) 895-2585
 8440 Allison Pointe Blvd., Suite 200, Indianapolis, IN 46250

November 24, 2020

TO: Stormwater Coordinator
 Clark County MS4
 501 East Court Street
 Jeffersonville, IN 47130

FROM: Michael S. Oliphant
 United Consulting
 8440 Allison Pointe Blvd.
 Indianapolis, IN 46250

RE: County Road 403 at Stacy Road
 Intersection Improvement Project
 Clark County, Indiana
 Des. No: 1802805

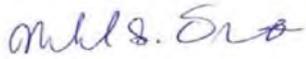
Proposed Project:

Officials with Clark County intend to proceed with a project at the intersection of County Road 403 and Stacy Road. (DES. No.: 1802805). You are being notified because this project lies within an Urbanized Area Boundary (UAB). In accordance with 327 IAC 15-13 (Rule 13 - Municipal Separate Storm Sewer Systems), INDOT has developed a Storm Water Quality Management Plan (SWQMP).

As part of its implementation, projects falling within the UAB will be required to consider appropriate post construction storm water quality best management practices (BMPs). These BMPs should take into consideration the available space, pollutants of concern and receiving waters.

This letter is for notification purposes only, and no action is required by you; however, if you would like to provide your input on water quality concerns, please provide this information **within thirty (30) calendar days** from the date of this letter to the undersigned. Should we not receive your response within the specified timeframe, it will be assumed that your agency does not have additional concerns about water quality issues resulting from the proposed project. Should you find that an extension to the response time is necessary, a reasonable amount of time may be granted upon request. If you have any questions regarding this matter, please feel free to contact Michael S. Oliphant, (317) 895-2585 / mike.oliphant@ucindy.com. Thank you in advance for your attention to this matter.

Sincerely,
UNITED CONSULTING


 Michael S. Oliphant, AICP
 Environmental Specialist

enclosures: Location Maps
 Ground Level Photographs

Appendix D

Section 106

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECTS
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
COUNTY ROAD 403 AND STACY ROAD INTERSECTION IMPROVEMENT PROJECT
CHARLESTOWN, CHARLESTOWN TOWNSHIP, CLARK COUNTY, INDIANA
DES. NO.: 1802805**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

The Area of Potential Effects (APE) is at least one property deep on each side of the roadway through the project area. The APE was widened in areas with greater viewshed to the project area. A map of the APE can be found in Appendix A.

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

The APE contains no properties listed, or eligible for listing, in the National Register of Historic Places (NRHP).

EFFECT FINDING

INDOT, acting on FHWA's behalf, has determined a "No Historic Properties Affected" finding is appropriate for this undertaking. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

This undertaking will not convert property from any Section 4(f) historic property to a transportation use; the INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Historic Properties Affected"; therefore no Section 4(f) evaluation is required.

Susan R. Branigin for  Digitally signed by Susan R. Branigin for
Date: 2020.11.17 09:38:41 -05'00'

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO HISTORIC PROPERTIES AFFECTED
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR SECTION 800.4(d)(1)
COUNTY ROAD 403 AND STACY ROAD INTERSECTION IMPROVEMENT PROJECT
CHARLESTOWN, CHARLESTOWN TOWNSHIP, CLARK COUNTY, INDIANA
DES. NO.: 1802805**

1. DESCRIPTION OF THE UNDERTAKING

A) Project Description

Clark County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the CR 403 and Stacy Road Intersection Improvement, in the City of Charlestown, Charlestown Township, Clark County, Indiana (Des. No. 1802805). The proposed undertaking is at the intersection of CR 403 (Old SR 403) and Stacy Road in Clark County, Indiana. It is within the City of Charlestown, Charlestown Township, Charlestown, IN-KY, 7.5' USGS Topographic Quadrangle, at the intersection of Sections 113, 114, 132, and 133, of Clark's Military Grant.

Both CR 403 and Stacy Road are two-lane roadways with narrow shoulders and no sidewalks, with drainage ditches present on one or both sides of the road. County Road 403 is generally an east-west route that was formerly known as Old Indiana 403. Originally known as Charlestown Road, CR 403 was an early road that connected Charlestown east to the larger market towns of Sellersburg and then south to New Albany. Stacy Road is a north-south route that begins at SR 62 and terminates at CR 403, a distance of approximately 3.5 miles. See Appendix A for maps of the project location.

The purpose of the project is to reduce the accidents at the intersection of CR 403 (Old SR 403) and Stacy Road. The need for the project is based in the accident history associated with this intersection. Accidents were studied during a 3-year period from 2014 to 2016 and 27 total accidents were reported. The 27 crashes are higher than the average number of crashes for this type of intersection. The Index of Crash Frequency (ICF) for all crash types at the intersection of Stacy Road and County Road 403 is 5.19 standard deviations higher than the expected number of crashes. Manner of collisions were primarily 55% rear end type collisions, 15% ran off road, and with 30% classified as other, including turning collisions.

The preferred alternative will upgrade the existing intersection to a single-lane roundabout. The proposed roundabout is expected to have a diameter of 134 feet. The approaches along CR 403 (Old SR 403) and Stacy Road will include an 18-foot circulatory lane. A standard 8-foot truck apron will be provided. Located in the southwest quadrant of the intersection, the Pleasant Grove Methodist Church parking lot ingress/egress

will be adjusted to ensure safe movement through the roundabout while maintaining access to the parking lot.

New curb turnouts will be constructed, as needed, along the proposed curb lines with revetment riprap connecting to the existing roadside ditches. This allows the existing open ditch drainage to function as originally designed with minimal disturbance; therefore, storm sewer design has not been included in this project scope. An additional 1.2 acres of permanent and 0.07 acre of temporary right-of-way will be required to construct the project.

The intersection will be closed during construction and detour routes will be posted. Salem-Noble Road to the west, Bethany Road to the east, and High Jackson Road to the south will provide alternate routes for traffic while the intersection is closed. The project letting date is March 2022.

Federal-aid highway construction projects qualify as “undertakings” as defined in CFR 800.16(y) and are subject to a Section 106 review as per Section 106 of the National Historic Preservation Act of 1966. Federal-aid funds will be used for planning and/or construction of the proposed rest area improvements; thus, a Section 106 review is applicable.

The Area of Potential Effects (APE) includes all locations where the project may result in disturbance of the ground; all locations from which elements of the project may be visible or audible; all locations where activity may result in changes in traffic patterns, land use, or public access; and all areas where there may be direct or indirect effects due to elements of the project. The archaeological APE has been defined as the project footprint. The above-ground structures APE is at least one property deep, and it was delineated based upon direct and indirect impacts, such as lane closures, visual and auditory changes, and changes to traffic patterns. Aerial maps of the APE are located in Appendix A and project site photographs are located in Appendix C.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

A) Historic Properties Report

To determine the presence of historic properties within the project’s APE a short-format historic property report (HPR) was prepared by Metric Environmental, LLC (Hudziak, 8/3/2020). Hudziak is a Qualified Professional (QP) and meets the Secretary of the Interior’s Professional Qualification Standards pursuant to 36 CFR Section 800.4(b). Hudziak reviewed historical resources such as the Indiana Register of Historic Sites and Structures and the National Register of Historic Places (NRHP) and found no previously listed properties within the APE.

Hudziak also reviewed the Clark County interim report (1988, Historic Landmarks Foundation of Indiana; resurveyed in 2010 by the Indiana Division of Historic Preservation and Archaeology), the State Historic Architectural and Archaeological Research Database (SHAARD) and the Indiana Historic

Buildings, Bridges, and Cemeteries Map (IHBBC), to determine past historical investigations conducted in the project area, and found that no previously surveyed properties were located within the APE.

Additionally, on June 30, 2020 Hudziak conducted a field survey of the project area.

The HPR included the project's APE and recommended that no properties within the APE are eligible for NRHP listing.

The Abstract and Conclusion sections of the HPR are presented in Appendix D. The INDOT Cultural Resources Office (INDOT-CRO) determined that the HPR was suitable for distribution to consulting parties on August 28, 2020.

B) Archaeological Survey

An Archaeological Phase Ia report was prepared by Metric Environmental, LLC (Snell, 8/19/20). Snell is a Qualified Professional (QP) archaeologist who meets the Secretary of the Interior's Professional Qualification Standards pursuant to 36 CFR Section 800.4(b). A literature review at the Department of Historic Preservation and Archaeology (DHPA) identified three archaeological and four site forms that have previously been recorded within 1.0 mile of the project.

On July 28, 2020, Snell and other Metric staff conducted field work that included a visual inspection, pedestrian survey, soil cores, and four shovel test probes in the project area. As a result of their investigation they determined the soil in the project area was previously disturbed by road and residential construction activity and buried utilities, and located no archaeological resources.

Snell prepared and submitted a report to the INDOT's CRO for review. The report recommended the project be allowed to proceed with no additional work. On September 14, 2020, the CRO released the report for submittal to the State Historic Preservation Office (SHPO) and to the appropriate Native American tribes for review and comment. The Archaeology Report Summary and Recommendations are presented in Appendix E.

C) Consultation

1. Consulting Party Invitation

Per 36 CFR 800.2(c)(1) the Federal agency fulfilling the Section 106 requirements is statutorily obligated to involve stakeholders in consultation. Per Section 101 (b)(3) of the National Historic Preservation Act of 1966, the SHPO is responsible for consulting on Federal undertakings that may affect historic properties.

In addition to the SHPO, the parties listed below were invited to participate as consulting parties for this undertaking. The parties were requested to indicate whether they agreed or did not agree to participate as a consulting party within 30 days of receipt of the invitation. It was noted that if the invited consulting party did not reply, they would not be considered a consulting party and would not receive further information about the undertaking unless the scope changed.

Invited Consulting Party	Accepted/Declined Invitation
Dr. Treva Hodges, Charlestown Mayor	No Response
Jack Coffman, Clark County Commissioner	No Response
Connie Sellers, Clark County Commissioner	No Response
Bryan Glover, Clark County Commissioner	No Response
R. Scott Lewis, Clark County Commissioner	No Response
Indiana Landmarks, Southern Regional Office	No Response
Jasper County Historical Society	No Response
Jeanne Burke, Clark County Historian/Clark County History Museum	No Response
Donna Hart, Charlestown Architectural Preservation Society	No Response
Jarrett Haley, Kentuckiana Regional Planning and Development Agency	No Response
Eastern Shawnee Tribe of Oklahoma	No Response
Shawnee Tribe	No Response
Miami Tribe of Oklahoma	Accepted
Peoria Tribe of Indiana of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	No Response
United Keetoowah Band of Cherokee Indians	No Response
Delaware Tribe of Indians	No Response

A hard copy of the Early Coordination Letter (ECL) was sent to the SHPO on May 21, 2020, and the other consulting parties received it via email. The SHPO was requested to identify the need to include additional consulting parties.

In a letter dated June 1, 2020 the SHPO acknowledged receipt of the ECL and noted they were not aware of any further stakeholders who should be invited to be consulting parties.

In a letter dated June 4, 2020 the Miami Tribe of Oklahoma accepted the invitation to participate as a consulting party and stated that if any Native American artifacts are discovered during the project their office should be notified immediately.

No other replies were received.

All consulting parties' correspondence is in Appendix F.

2. Consultation Regarding the Area of Potential Effects and National Register of Historic Places Eligibility

On September 14, 2020 a request to review the HPR and the Archaeology Report was sent to the SHPO and the consulting parties that accepted the May 21, 2020 invitation. The SHPO was provided hard

copy of both reports, and the other consulting parties were notified of the availability of the HPR for review at IN SCOPE (<http://erms.indot.in.gov/Section106Documents/>). Recipients of this review request were asked to provide comments within 30 days.

In a letter dated September 29, 2020 the SHPO acknowledged receipt of the HPR and Archaeology Reports. The SHPO agreed with the HPR's proposed APE and recommendations. The SHPO concurred with the opinion of the archaeologist that no further archaeological investigations are necessary. The SHPO recommended INDOT to put forth a finding.

No other replies were received.

All consulting parties' correspondence is in Appendix F.

3. BASIS FOR FINDING

The SHPO concurred with the proposed APEs and NRHP eligibility recommendations of both the project historian and the archaeologist. No other consulting parties provided comment.

Because no historic properties are present within the APE, a finding of "no historic properties affected" has been made for this undertaking.

INDOT's Findings, made on behalf of the FHWA, and supporting 800.11(d) documentation are hereby provided to the SHPO and other consulting parties for a final 30-day comment period. Views of the public are being concurrently sought through publication of the Findings in a locally available, widely circulated newspaper. This document will be revised if necessary if public comment warrants it.

APPENDICES

- A. Project Location Maps and APE
- B. Project Information and Plans
- C. Project Site Photographs and Key Maps
- D. Historic Property Report Summary and Conclusion
- E. Archaeology Report Summary and Recommendations
- F. Consulting Parties' Correspondence

APPENDIX A: Project Location Maps and APE



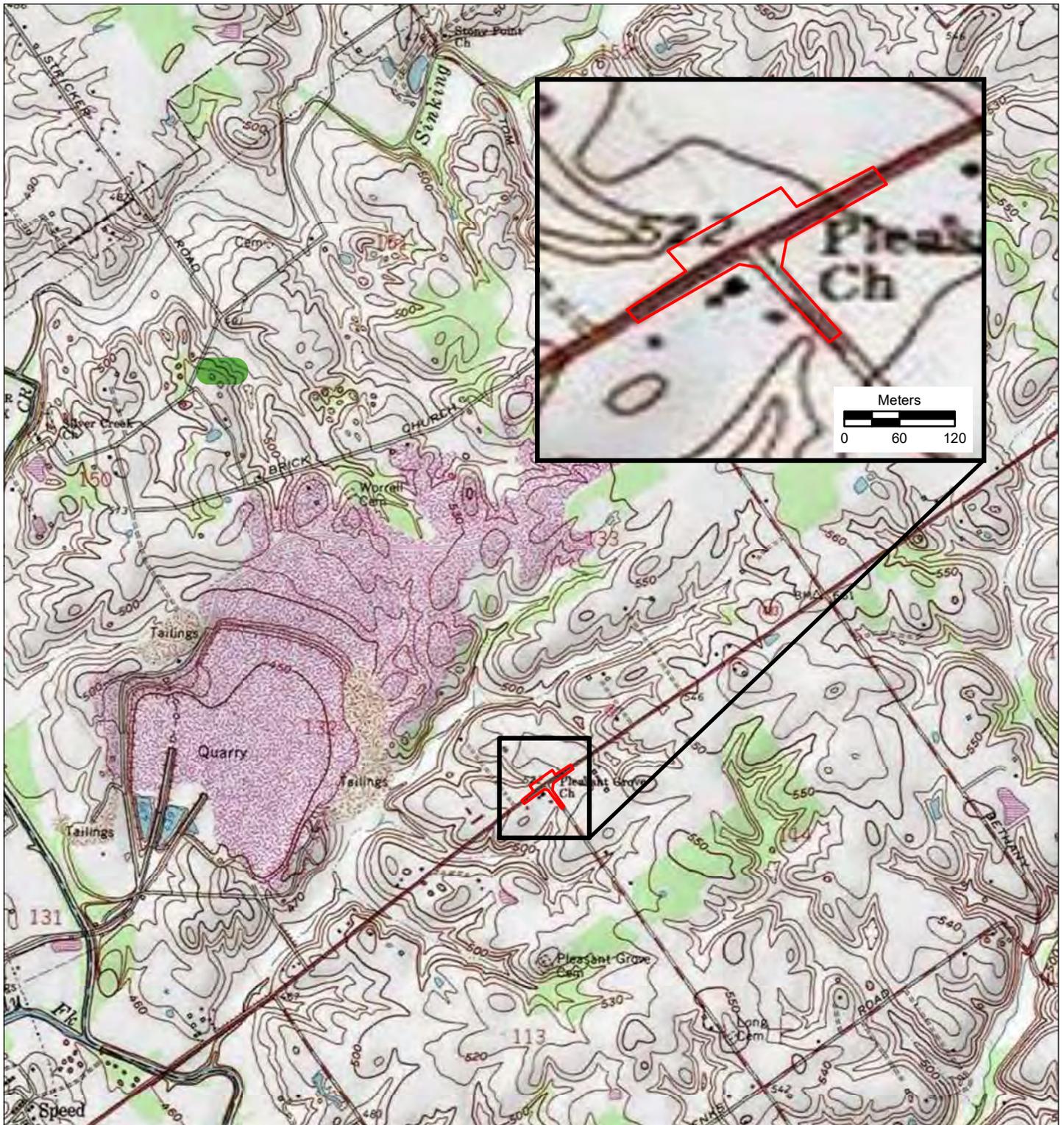
- Counties
- Clark County
- Indianapolis

Figure 1. Project area location
 CR 403 (Old SR 403) and Stacy Road
 Intersection Improvement
 Charlestown Township, Clark County, Indiana
 Des. No. 1802805
 Metric Project No. 19-0055
 Map Date: 08/03/2020



1 cm = 25 km





 Survey Area

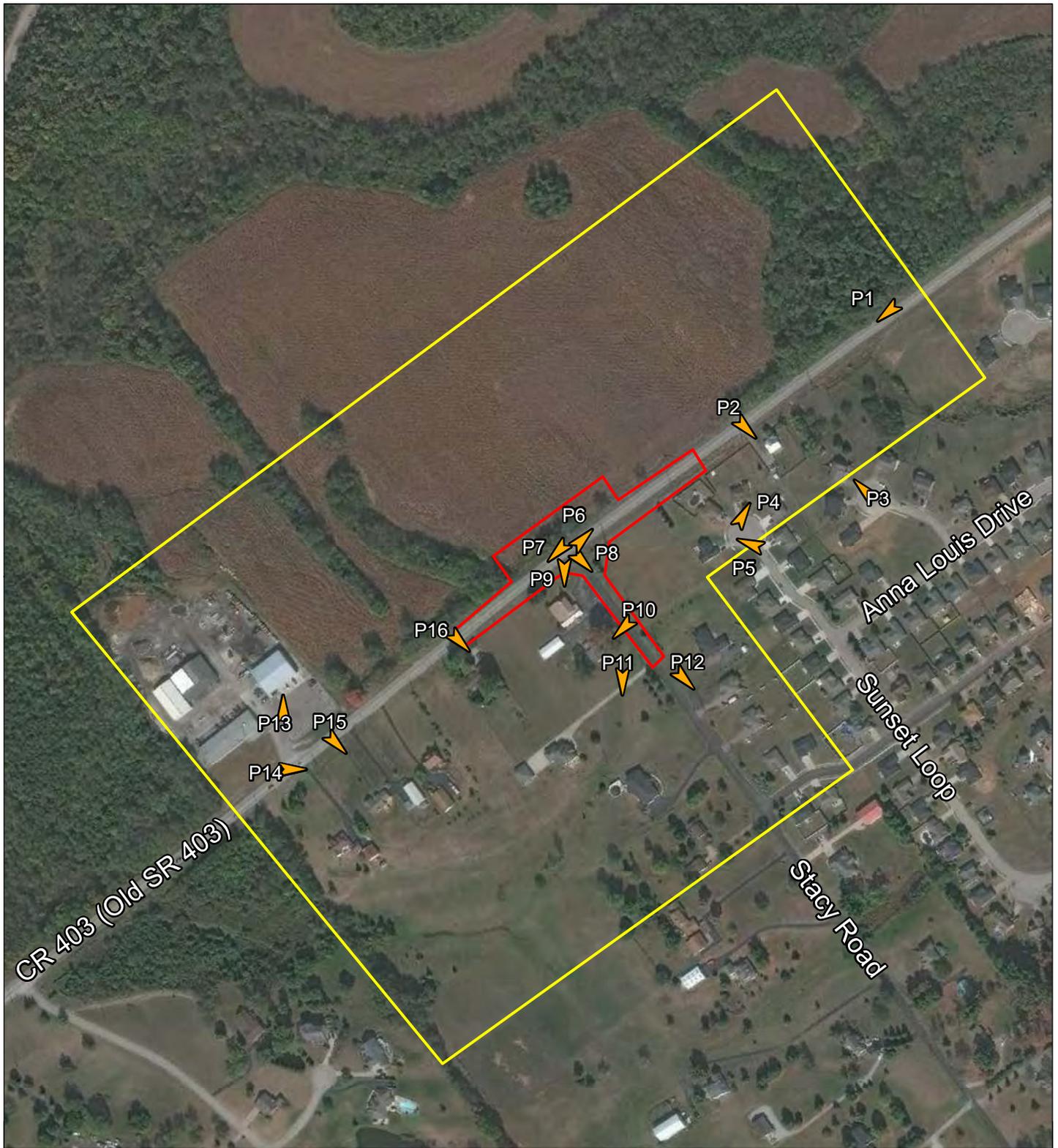
Figure 2. Project area on a portion of the 1981 Charlestown, IN, 7.5 Minute Quadrangle CR 403 (Old SR 403) and Stacy Road Intersection Improvement Charlestown Township, Clark County, Indiana Des. No. 1802805 Metric Project No. 19-0055 Map Date: 08/03/2020

All Locations Approximate 1981 Basemap



1 cm = 240 m





- Survey Area
- APE
- ▶ Photograph Location

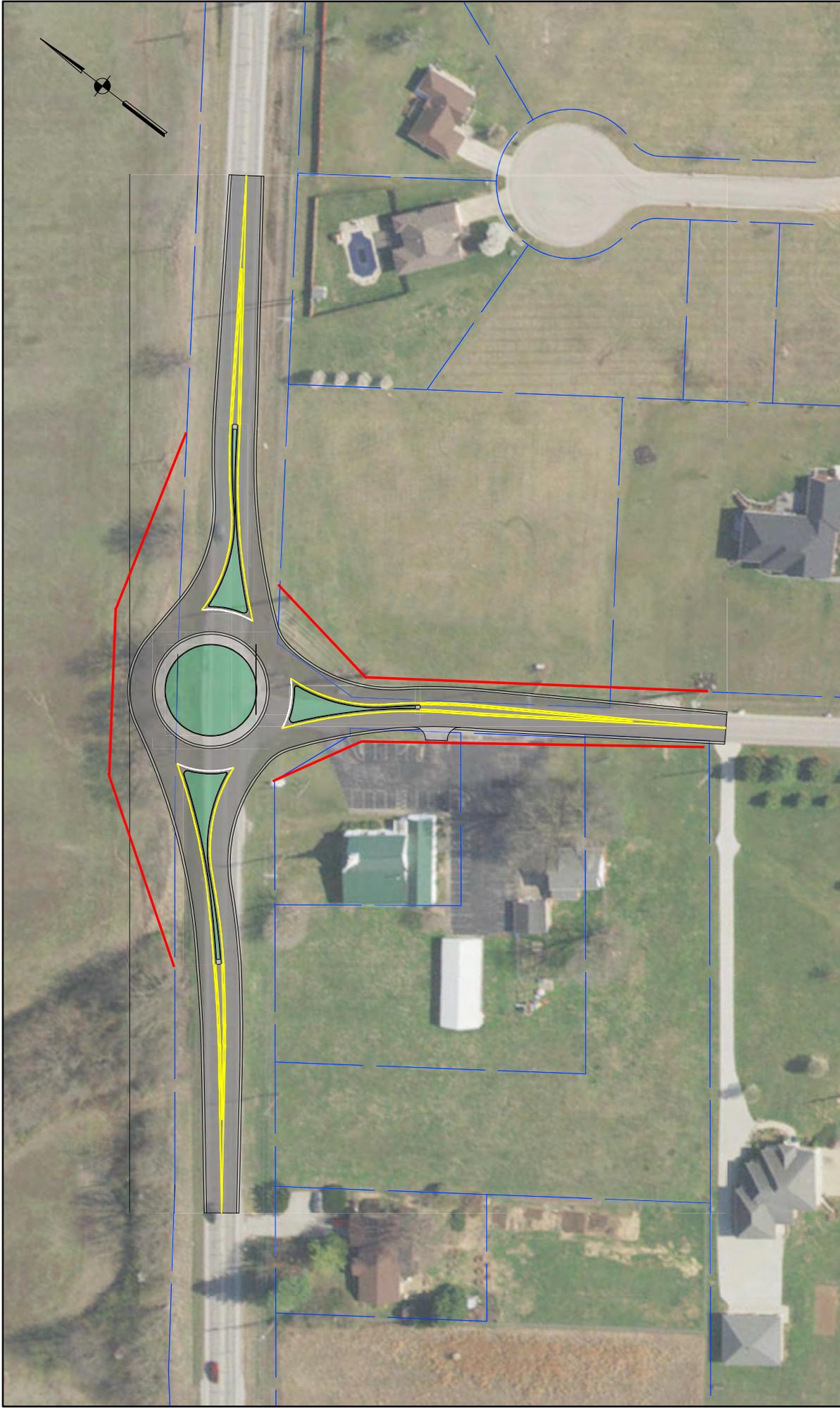
Figure 2. Project area, APE, and photograph locations on an aerial photograph CR 403 (Old SR 403) and Stacy Road Intersection Improvement Charlestown Township, Clark County, Indiana Des. No. 1802805 Metric Project No. 19-0055 Map Date: 07/28/2020

All Locations Approximate
 2013 Basemap
 Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

1 inch = 400 feet
 Feet



APPENDIX B: Project Information and Plans



8440 Allison Pointe Blvd.
 Suite 200
 Indianapolis, IN 46250
 Phone 317-895-2585
 www.uneity.com

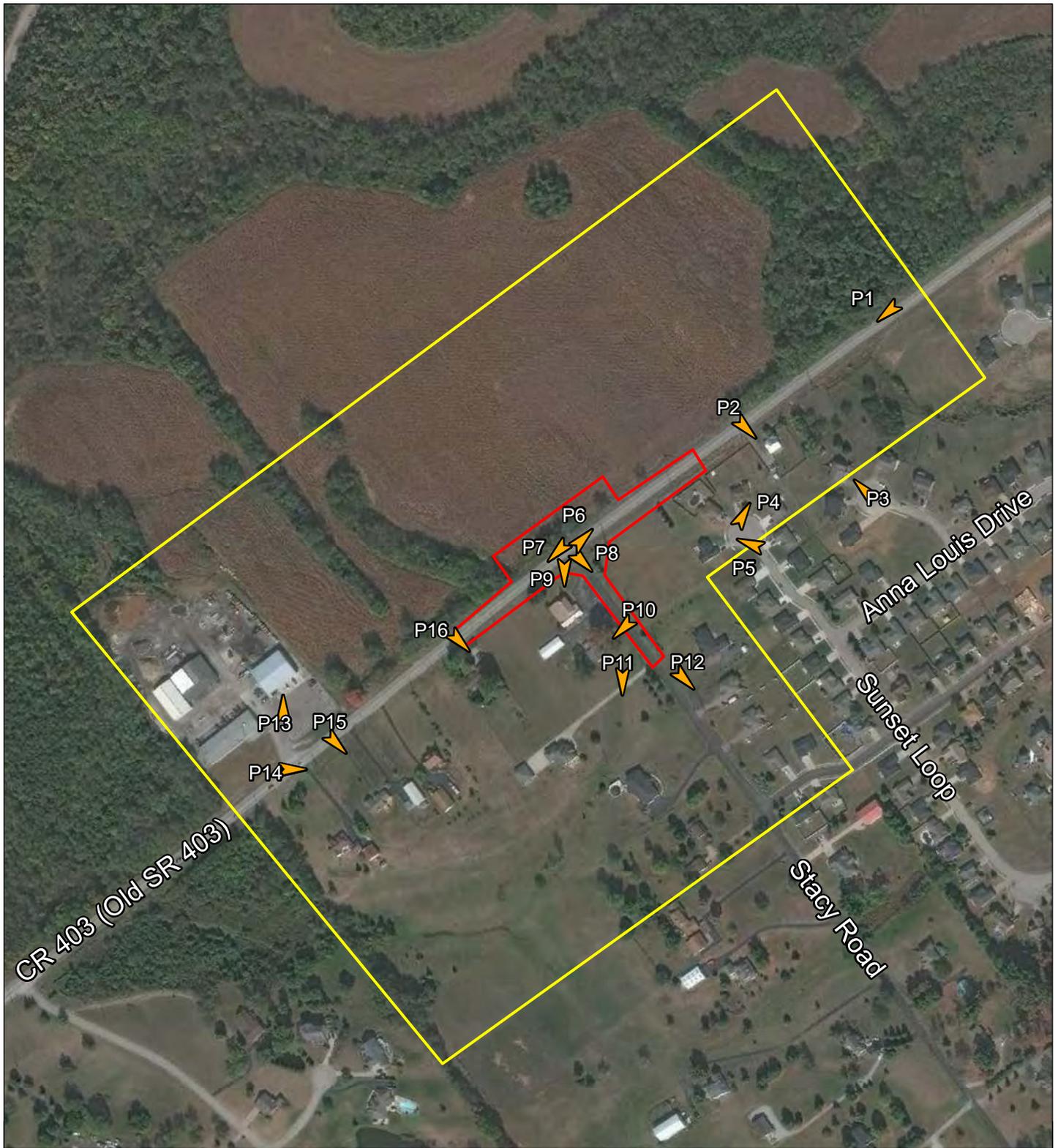


SCALE

CLARK COUNTY HIGHWAY DEPARTMENT
 OLD SR 403 & STACY ROAD IMPROVEMENTS

PROPOSED ROUNDABOUT

APPENDIX C: Project Site Photographs and Key Maps

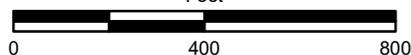


- Survey Area
- APE
- ▶ Photograph Location

Figure 2. Project area, APE, and photograph locations on an aerial photograph CR 403 (Old SR 403) and Stacy Road Intersection Improvement Charlestown Township, Clark County, Indiana Des. No. 1802805 Metric Project No. 19-0055 Map Date: 07/28/2020

All Locations Approximate
 2013 Basemap
 Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

1 inch = 400 feet
 Feet



APPENDIX

PHOTOGRAPHS OF THE APE



Photo 1. County Road 403 facing SW from 6816 County Road 403 toward project intersection.



Photo 2. 6816 County Road 403 facing SE (non-contributing, or NC).



Photo 3. 6401 Miners Pointe facing NW (NC).



Photo 4. 6303 Sunset Loop facing NE (NC).



Photo 5. 6302 and 6300 Sunset Loop facing NW (NC).



Photo 6. View along County Road 403 intersecting Stacy Road facing NE.



Photo 7. View along County Road 403 intersecting Stacy Road facing SW.



Photo 8. View along Stacy Road intersecting County Road 403 facing SE.



Photo 9. 8211 Stacy Road facing south (NC).



5

Photo 10. 8207 Stacy Road facing SW (NC).



Photo 11. View of houses on the west side of Stacy Road north of County Road 403 facing south (NC).



Photo 12. View of houses on the east side of Stacy Road north of County Road 403 facing southeast (NC).



Photo 13. 6103 County Road 403 facing north (NC).



Photo 14. View along County Road 403 facing east toward the project intersection.



Photo 15. View of houses on the south side of County Road 403 facing SE (NC).



Photo 16. 6208 County Road 403 facing SW (NC).

APPENDIX D: Historic Properties Report Summary and Conclusions

HISTORIC PROPERTY SHORT REPORT

CR 403 AND STACY ROAD INTERSECTION IMPROVEMENT PROJECT
DES NO. 1802805
CHARLESTOWN TOWNSHIP, CLARK COUNTY, INDIANA

PREPARED FOR:

UNITED CONSULTING

LEAD AGENCY:

FEDERAL HIGHWAY ADMINISTRATION

Prepared by:



Complex Environment. Creative Solutions.

6971 Hillsdale Court
Indianapolis, IN 46256
Telephone: 317.400.1633
www.metricenv.com

Candace Hudziak

Candace Hudziak
candaceh@metricenv.com

August 3, 2020

MANAGEMENT SUMMARY

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the proposed CR 403 and Stacy Road Intersection Improvement Project in Charlestown Township, Clark County, Indiana. Aboveground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (NRHP). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed in the NRHP. The APE contains no properties that are recommended eligible for listing in the NRHP.

5.0 CONCLUSIONS

The APE contains no properties listed in the NRHP.

As a result of identification and evaluation efforts for this project, no properties are recommended eligible for listing in the NRHP.

APPENDIX E: Archaeology Report Summary and Recommendations

ARCHAEOLOGICAL SHORT REPORT

PHASE IA ARCHAEOLOGICAL SURVEY FOR THE CR 403 (OLD SR 403) AND STACY ROAD INTERSECTION IMPROVEMENT, DES. NO.1802805, CHARLESTOWN, CHARLESTOWN TOWNSHIP, CLARK COUNTY, INDIANA.

PREPARED FOR:

UNITED CONSULTING

LEAD AGENCY:

CLARK COUNTY

AUGUST 19, 2020

Prepared by:

Megan Copenhaver and Sydney Heidenreich



Complex Environment. Creative Solutions.

6971 Hillside Court
Indianapolis, IN 46256
Telephone: 317.400.1633
www.metricenv.com

A handwritten signature in cursive script that reads "Samuel P. Snell".

Samuel P. Snell, MS, RPA
Archaeological Principal Investigator
sams@metricenv.com
August 19, 2020



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES DIVISION OF HISTORIC PRESERVATION AND ARCHAEOLOGY

402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Megan Copenhaver and Sydney Heidenreich

Date (month, day, year): August 19, 2020

Project Title: Phase Ia Archaeological Survey for the CR 403 (Old SR 403) and Stacy Road Intersection Improvement Des. No. 1802805, Charlestown, Charlestown Township, Clark County, Indiana.

PROJECT OVERVIEW

Project Description:

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the County Road (CR) 403 and Stacy Road Intersection Improvement Project (Des No. 1802805). The project area is located in the northwestern city limits of Charlestown, in Charlestown Township, Clark County, Indiana. The project can be found on the Charlestown, Indiana-Kentucky 7.5-minute series USGS Topographic Quadrangle map at the intersection of Sections 113, 114, 132, and 133 of Clark's Military Grant.

The preferred alternative will upgrade the existing intersection to a single-lane roundabout. The proposed roundabout is expected to have a diameter of 134 feet. The approaches along CR 403 (Old SR 403) and Stacy Road will include an 18-foot circulatory lane. A standard 8-foot truck apron will be provided. The project area is 259.3 m (850.7 ft) long and 48 m (157.5 ft) wide at the widest along CR 403 and is 121.7 m (399.2 ft) long and 14.1 m (46.2 ft) wide along Stacy Road. The project area encompasses 1.1 ha (2.6 ac).

INDOT Designation Number/ Contract Number: 1802805 Project Number:

DHPA Number: 25632 Approved DHPA Plan Number:

Prepared For: United Consulting

Contact Person: Mr. Devin L. Stettler, MPI, AICP, Environmental Services Manager

Address: 8440 Allison Pointe Boulevard, Suite 200

City: Indianapolis State: IN ZIP Code: 46250

Telephone Number: 317.895.2585 Email Address: Devin.Stettler@ucindy.com

Principal Investigator: Samuel P. Snell, MS, RPA

Signature:

Company/Institution: Metric Environmental

Address: 6971 Hillside Court

City: Indianapolis State: IN ZIP Code: 46250

The project area is 259.3 m (850.7 ft) long and 48 m (157.5 ft) wide at the widest along CR 403 and is 121.7 m (399.2 ft) long and 14.1 m (46.2 ft) wide along Stacy Road. The project area encompasses 1.1 ha (2.6 ac) [Figure 1-18].

Comments:

Area 1 consisted of the residential and church property located on the southeastern side of CR 403 (Figures 4-12). The section of Area 1 located east of the Stacy Road and CR 403 intersection was shown to be completely disturbed in a 2002 aerial taken from Google Earth due to the construction of residential properties and several buried utility lines. A roadside ditch extended along CR 403 within this quadrant of the project area and a layer of gravel secured a line of telephone poles eight meters (26.2 ft) from the edge of the pavement. No STPs were excavated or soil cores advanced within this quadrant of Area 1 because of the observed disturbances. The southwestern quadrant of Area 1 located west of Stacy Road was disturbed due to buried utilities, residential driveways, a drainage ditch and the Pleasant Grove Church parking lot. No STPs or soil cores were excavated along the southwestern side of Stacy Road due to these disturbances. A shallow roadside ditch ran along CR 403 in this quadrant of Area 1. Three soil cores were advanced at 30 meter intervals along CR 403 and the profiles were the same with a brown (10YR4/2) clay mixed with a dark yellowish brown (10YR3/6) clay subsoil extending 35 cm (13.7 in) underlain by a dark yellowish brown (10YR3/6) clay subsoil.

Area 2 consisted of wooded terrain and an agricultural field located on the northwest side of CR 403 (Figures 13-18). A steep roadside ditch that ran along CR 403 for the entire length of the project area was visually inspected. An agricultural field containing soybeans was excavated by pedestrian survey in 6 transects walked at 5 meter (16.4 ft) intervals. The visibility for this agricultural field was 50 percent. Four negative STPs were excavated in a narrow wooded area approximately 45 meters (147.6 ft) long and 8 meters (26.2 ft) wide located on the northwestern side of the intersection of CR 403 and Stacy Road. The STP 1 contained a brown (10YR4/2) clay mixed with a dark yellowish brown (10YR3/6) clay subsoil extending 25 cm (9.8 in) underlain by a dark yellowish brown (10YR3/6) clay subsoil. The other three STPs contained eroded soils with subsoil at the surface. This wooded area also contained several telephone poles, a fire hydrant and a sewer cap.

No archaeological sites were located during the survey and no further archaeological work is recommend for this project.

Recommendation

- The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

In the unlikely event that archaeological deposits or human remains are encountered during the construction phase of the project, all work must cease and archaeologists from the DHPA and the INDOT-CRO must be notified.

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

APPENDIX F: Consulting Parties' Correspondence



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

May 21, 2020

This letter was sent to the listed parties.

RE: CR 403 (Old SR 403) and Stacy Road Intersection Improvement Des. No. 1802805, Charlestown, Charlestown Township, Clark County, Indiana.

Dear Consulting Party (see attached list),

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the CR 403 (Old SR 403) and Stacy Road Intersection Improvement Des. No. 1802805, Charlestown, Charlestown Township, Clark County, Indiana. Metric Environmental, LLC. is under contract with United Consulting and Clark County to advance the environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is at the intersection of CR 403 (Old SR 403) and Stacy Road in Clark County, Indiana. It is within the City of Charlestown, Charlestown Township, Charlestown, IN-KY, 7.5' USGS Topographic Quadrangle, at the intersection of Sections 113, 114, 132, and 133, of Clark's Military Grant.

The primary purpose of the project is to reduce the accidents at the intersection of CR 403 (Old SR 403) and Stacy Road. The need for the project is based in the accident history associated with this intersection. Accidents were studied during a 3-year period from 2014 to 2016 and 27 total accidents were reported. The 27 crashes are higher than the average number of crashes for this type of intersection. The Index of Crash Frequency (ICF) for all crash types at the intersection of Stacy Road and County Road 403 is 5.19 standard deviations higher than the expected number of crashes. Manner of collisions were primarily 55% rear end type collisions, 15% ran off road, and with 30% classified as other, including turning collisions.

The preferred alternative will upgrade the existing intersection to a single-lane roundabout. The proposed roundabout is expected to have a diameter of 134 feet. The approaches along CR 403 (Old SR 403) and Stacy Road will include an 18-foot circulatory lane. A standard 8-foot truck apron will be provided. Located in the southwest quadrant of the intersection, the Pleasant Grove Methodist Church parking lot ingress/egress will be adjusted to ensure safe movement through the roundabout while maintaining access to the parking lot. All geometric elements presented are preliminary and will be verified during the design process. Thus, they are subject to change.

New curb turnouts will be constructed, as needed, along the proposed curb lines with revetment riprap connecting to the existing roadside ditches. This allows the existing open ditch drainage to function as originally designed with minimal disturbance; therefore, storm sewer design has not been included in this project scope. Additional permanent and temporary right-of-way will be required to construct the project. The exact amount is not yet known and will be developed as the design advances.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

www.in.gov/dot/
An Equal Opportunity Employer



The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

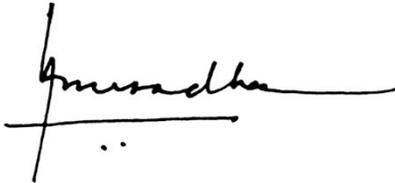
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Samuel P. Snell of Metric Environmental, LLC., at 317-912-3499 or sams@Metricenv.com. All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC. at the following address:

Samuel P. Snell
Archaeological Principal Investigator
Metric Environmental, LLC.
6971 Hillside Court
Indianapolis, Indiana 46250
sams@metricenv.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:
Location Map
Aerial Map

****Note: the topographic maps, project limits maps, and project plan sheets, can be found in Appendix A and B of this document***

Distribution List:

State Historic Preservation Officer
Clark County Historian
Clark County Commissioners
Charlestown Mayor
Miami Tribe of Oklahoma
Pokagon Band of Potawatomi Indian
Delaware Tribe of Indians

Indiana Landmarks, Southern Regional Office
Kentuckiana Regional Planning and Development Agency
Charlestown Architectural Preservation Society
Eastern Shawnee Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
United Keetoowah Band of Cherokee Indians
Shawnee Tribe

From: Sam Snell <sams@metricenv.com>
Subject: FHWA Project: Des. No. 1802805; CR 403 (Old SR 403) and Stacy Road Intersection Improvement, Charlestown, Charlestown Township, Clark County, Indiana
Date: May 21, 2020 at 10:26:35 AM EDT
To: "gsekula@indianalandmarks.org" <gsekula@indianalandmarks.org>, "Jeanne_b@hotmail.com" <Jeanne_b@hotmail.com>, "jeannemburke5807@gmail.com" <jeannemburke5807@gmail.com>, "Jarrett.haley@kipda.org" <Jarrett.haley@kipda.org>, "jcoffman@co.clark.in.us" <jcoffman@co.clark.in.us>, "csellers@co.clark.in.us" <csellers@co.clark.in.us>, "bglover@co.clark.in.us" <bglover@co.clark.in.us>, "slewis@co.clark.in.us" <slewis@co.clark.in.us>, "mayor@cityofcharlestown.com" <mayor@cityofcharlestown.com>
Cc: "Ross, Anthony" <ARoss3@indot.IN.gov>

Des. No.: 1802805

Project Description: CR 403 (Old SR 403) and Stacy Road Intersection Improvement

Location: Charlestown, Charlestown Township, Clark County, Indiana

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the CR 403 (Old SR 403) and Stacey Road Intersection Improvement Des. No1802805, Charlestown, Charlestown Township, Clark County, Indiana.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer
Indiana Landmarks, Southern Regional Office
Clark County Historian
Clark County Commissioners
Kentuckiana Regional Planning and Development Agency
Charlestown Architectural Preservation Society
Charlestown Mayor

Eastern Shawnee Tribe of Oklahoma
Shawnee Tribe
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indian
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Samuel P. Snell, MS, RPA

Archaeological Principal
Investigator



Phone: 317.912.3499 Email:
sams@metricenv.com
6971 Hillside Court, Indianapolis, IN 46250



www.metricenv.com



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GARY | CINCINNATI



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From: [Ross, Anthony](#)
To: thpo@estoo.net; [Diane Hunter](#); lpappenfort@peoriatribe.com; [Matthew Bussler \(Matthew.Bussler@pokagonband-nsn.gov\)](#); tonya@shawnee-tribe.com; wwarrior@ukb-nsn.gov; lheady@delawaretribe.org
Cc: [Sam Snell](#); [Miller, Shaun \(INDOT\)](#); [Allen, Michelle \(FHWA\)](#); [Branigin, Susan](#); [Kumar, Anuradha](#)
Subject: FHWA Project: Des. No. 1802805; CR 403 (Old SR 403) and Stacy Road Intersection Improvement, Charlestown, Charlestown Township, Clark County, Indiana
Date: Thursday, May 21, 2020 4:00:34 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image006.png](#)
[CR403StacyRoad_Des1802805_EarlyCoordination_2020-05-21.pdf](#)

External Message: *This message originated outside of Metric Environmental.
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Des. No.: 1802805

Project Description: CR 403 (Old SR 403) and Stacy Road Intersection Improvement

Location: Charlestown, Charlestown Township, Clark County, Indiana

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the CR 403 (Old SR 403) and Stacey Road Intersection Improvement Des. No.1802805, Charlestown, Charlestown Township, Clark County, Indiana.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer
Indiana Landmarks, Southern Regional Office
Clark County Historian
Clark County Commissioners
Kentuckiana Regional Planning and Development Agency
Charlestown Architectural Preservation Society
Charlestown Mayor
Eastern Shawnee Tribe of Oklahoma
Shawnee Tribe
Miami Tribe of Oklahoma
Peoria Tribe of Indians of Oklahoma
Pokagon Band of Potawatomi Indian
United Keetoowah Band of Cherokee Indians
Delaware Tribe of Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Anthony Ross, Ph.D.

LPA Program Administrator

Cultural Resources Office

Environmental Services

100 N. Senate Ave., Room N642-ES

Indianapolis, IN 46204

Office: (317) 234-0142

Email: gross3@indot.in.gov





June 1, 2020

Samuel P. Snell
Metric Environmental, LLC
6971 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Early coordination letter for the CR 403 (old SR 403) and Stacy Road Intersection Improvement
project in Charlestown, Clark County, Indiana (Des. No. 1802805; DHPA No. 25632)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed your May 21, 2020 review request submittal form which enclosed INDOT’s early coordination letter, which we received the same day for the aforementioned project.

We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. However, if right-of-way is likely to be taken from a potentially historic property, it might be advisable to invite the owner of that property as soon as possible. In your next regular correspondence on this project, please advise us as to which of the invited consulting parties has accepted the invitation.

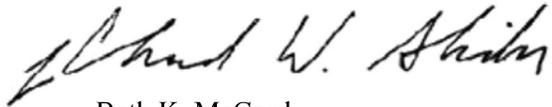
We look forward to reviewing the proposed area of potential effects and the reports on investigations of above-ground cultural resources and archaeological resources that the early coordination letter indicated will be forthcoming.

The Indiana SHPO staff’s archaeological reviewer for this project is Rachel Sharkey, and the structures reviewer is Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

Samuel P. Snell
June 1, 2020
Page 2

In all future correspondence about the CR 403 (old SR 403) and Stacy Road Intersection Improvement project in Charlestown, Clark County (Des. No. 1802805), please refer to DHPA No. 25632.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Anthony Ross, INDOT
Samuel Snell, Metric Environmental, LLC
Rachel Sharkey, INDNR-DHPA
Danielle Kauffmann, INDNR-DHPA



Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355
Ph: (918) 541-1300 • Fax: (918) 542-7260
www.miamination.com



Via email: smiller@indot.in.gov

June 2, 2020

Shaun Miller
Archaeological Team Lead, Cultural Resources Office
Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1802805; CR 403 and Stacy Road Intersection Improvement, Charlestown,
Charlestown Township, Clark County, Indiana – Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. The Miami Tribe of Oklahoma, a federally recognized Indian tribe with a Constitution ratified in 1939 under the Oklahoma Indian Welfare Act of 1936, respectfully submits the following comments regarding Des. No. 1802805.

The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

September 14, 2020

This letter was sent to the listed parties.

RE: County Road 40 and Stacy Road Intersection Improvement Project
Charlestown Township, Clark County, Indiana
Des. No. 1802805, DHPA No. 25632

Dear Consulting Party,

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with the CR 403 and Stacy Road Intersection Improvement Des. No. 1802805, Charlestown, Charlestown Township, Clark County, Indiana (Des. No. 1802805).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on May 21, 2020.

The proposed undertaking is at the intersection of CR 403 (Old SR 403) and Stacy Road in Clark County, Indiana. It is within the City of Charlestown, Charlestown Township, Charlestown, IN-KY, 7.5' USGS Topographic Quadrangle, at the intersection of Sections 113, 114, 132, and 133, of Clark's Military Grant.

The primary purpose of the project is to reduce the accidents at the intersection of CR 403 (Old SR 403) and Stacy Road. The need for the project is based in the accident history associated with this intersection. Accidents were studied during a 3-year period from 2014 to 2016 and 27 total accidents were reported. The 27 crashes are higher than the average number of crashes for this type of intersection. The Index of Crash Frequency (ICF) for all crash types at the intersection of Stacy Road and County Road 403 is 5.19 standard deviations higher than the expected number of crashes. Manner of collisions were primarily 55% rear end type collisions, 15% ran off road, and with 30% classified as other, including turning collisions.

The preferred alternative will upgrade the existing intersection to a single-lane roundabout. The proposed roundabout is expected to have a diameter of 134 feet. The approaches along CR 403 (Old SR 403) and Stacy Road will include an 18-foot circulatory lane. A standard 8-foot truck apron will be provided. Located in the southwest quadrant of the intersection, the Pleasant Grove Methodist Church parking lot ingress/egress will be adjusted to ensure safe movement through the roundabout while maintaining access to the parking lot. All geometric elements presented are preliminary and will be verified during the design process. Thus, they are subject to change.

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New curb turnouts will be constructed, as needed, along the proposed curb lines with revetment riprap connecting to the existing roadside ditches. This allows the existing open ditch drainage to function as originally designed with minimal disturbance; therefore, storm sewer design has not been included in this project scope. Additional permanent and temporary right-of-way will be required to construct the project. The exact amount is not yet known and will be developed as the design advances.

United Consulting is under contract with Clark County to advance the environmental documentation for the referenced project. Metric Environmental has been subcontracted to complete the Section 106 documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (NRHP).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the NRHP. As a result of the historic property identification and evaluation efforts, no above-ground resources are recommended as eligible for listing in the NRHP.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards conducted a Phase Ia investigation within the project area; however. As a result of these efforts, no archaeological sites were identified, and no further work is recommended.

The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard copy of this material, please respond to this email with your request within seven (7) days.

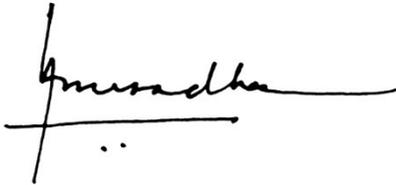
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Samuel P. Snell of Metric Environmental, LLC., at 317-912-3499 or sams@Metricenv.com. All future responses regarding the proposed project should be forwarded to Metric Environmental, LLC. at the following address:

Samuel P. Snell
Archaeological Principal Investigator
Metric Environmental, LLC.
6971 Hillside Court
Indianapolis, Indiana 46250
sams@metricenv.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Distribution List:
Indiana SHPO
Miami Tribe

Enclosures:
Topo map of the project location

**Note: the topographic maps, project limits maps, and project plan sheets, can be found in Appendix A and B of this document*

From: [Candace Hudziak](#)
To: [Sharkey, Rachel](#); dkauffmann@dnr.in.gov
Cc: [Ross, Anthony](#); [Moffatt, Charles D](#); [Miller, Shaun \(INDOT\)](#); [Branigin, Susan](#); [Devin Stettler](#); [Sam Snell](#); [Luella Beth Hillen](#)
Subject: FHWA Project: Des. No. 1802805; CR 403 (Old SR 403) and Stacy Road Intersection Improvement, Charlestown, Charlestown Township, Clark County, Indiana
Date: Monday, September 14, 2020 11:28:00 AM
Attachments: [CR403StacyRoad_Des1802805_DistLtrHPR_2020-09-14.pdf](#)
[image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)

Des. No.: 1802805

Project Description: CR 403 and Stacy Road Intersection Improvement

Location: Charlestown, Charlestown Township, Clark County, Indiana

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the CR 403 (Old SR 403) and Stacey Road Intersection Improvement Des. No.1802805, Charlestown, Charlestown Township, Clark County, Indiana. The Section 106 Early Coordination Letter was originally distributed on May 21, 2020.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Phase Ia Archaeological Report (Tribes only) have been prepared and are ready for review and comment by consulting parties.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Candace Hudziak
Senior Project Manager



Phone: 317.643.1633 Email: candaceh@metricenv.com
6971 Hillside Court, Indianapolis, IN 46250



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From: [Moffatt, Charles D](#)
To: thpo@estoo.net; [Diane Hunter](#); lpappenfort@peoriatribe.com; [Matthew Bussler \(Matthew.Bussler@pokagonband-nsn.gov\)](mailto:Matthew.Bussler@pokagonband-nsn.gov); tonya@shawnee-tribe.com; lheady@delawaretribe.org; egorsuch@ukb-nsn.gov
Cc: [Miller, Shaun \(INDOT\)](#); [Allen, Michelle \(FHWA\)](#)
Subject: FHWA Project: Des. No. 1802805; CR 403 (Old SR 403) and Stacy Road Intersection Improvement, Charlestown, Charlestown Township, Clark County, Indiana
Attachments: [CR403StacyRoad_Des1802805_DistLtrHPR_2020-09-14.pdf](#)

Des. No.: 1802805

Project Description: CR 403 and Stacy Road Intersection Improvement

Location: Charlestown, Charlestown Township, Clark County, Indiana

Clark County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with the CR 403 (Old SR 403) and Stacey Road Intersection Improvement Des. No.1802805, Charlestown, Charlestown Township, Clark County, Indiana. The Section 106 Early Coordination Letter was originally distributed on May 21, 2020.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Phase Ia Archaeological Report (Tribes only) have been prepared and are ready for review and comment by consulting parties.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

David Moffatt
Archaeologist
Environmental Services
Cultural Resources Office
Indiana Department of Transportation
317-233-3703



September 29, 2020

Samuel P. Snell
Metric Environmental, LLC
6971 Hillsdale Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Archaeological short report (Copenhaver/Heidenreich, 8/19/2020) and historic
property short report (Hudziak, 8/3/2020) for the CR 403 (Old SR 403) and Stacy Road
intersection improvement project in Charlestown, Clark County, Indiana (Des. No.
1802805; DHPA No. 25632)

Dear Mr. Snell:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed your September 14, 2020, review request submittal form, which enclosed the aforementioned reports, all of which we received the same day, for the aforementioned project.

The proposed area of potential effects (“APE”) appears to be of adequate size to encompass the geographic area in which direct and indirect effects of a project of this nature could occur.

For the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions of the historic property short report (“HPSR”; Hudziak, 8/3/2020) that there are no historic properties listed or eligible for inclusion in the National Register of Historic Places (“NRHP”) located within the project’s APE.

Based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area. We concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Copenhaver/Heidenreich, 8/19/2020), that no further archaeological investigations appear necessary at these portions of the proposed project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party expresses a different opinion, it might now be appropriate to ask INDOT for a finding.

The Indiana SHPO staff’s archaeological reviewer for this project is Rachel Sharkey and the structures reviewer is Danielle Kauffmann. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the CR 403 and Stacy Road intersection improvement in Charlestown, Clark County (Des. No. 1802805), please continue to refer to DHPA No. 25632.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:RAS:ras

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Anthony Ross, INDOT
Susan Branigin, INDOT
Samuel P. Snell, Metric Environmental LLC
Diane Hunter, Miami Tribe of Oklahoma
Danielle Kauffmann, INDNR-DHPA
Rachel Sharkey, INDNR-DHPA

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



December 7, 2020

Candace Hudziak
Metric Environmental, LLC
6971 Hillside Court
Indianapolis, Indiana 46250

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no historic properties affected” on behalf
of the Federal Highway Administration for the CR 403 (Old SR 403) and Stacy Road
intersection improvement project, Charlestown, Clark County, Indiana (Des. No. 1802805;
DHPA No. 25632)

Dear Ms. Hudziak:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), implementing regulations at 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO”) has reviewed your November 17, 2020 submission which enclosed INDOT’s finding and supporting documentation for the aforementioned project.

As previously indicated, we agree that there are no historic properties listed or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the project’s area of potential effects.

Also as previously indicated, based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area. We concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Copenhaver/Heidenreich, 8/19/2020), that no further archaeological investigations appear necessary at these portions of the proposed project area.

Accordingly, we concur with INDOT’s November 17, 2020, Section 106 finding of “No Historic Properties Affected” on behalf of FHWA for this federal undertaking.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the DNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The archaeological reviewer for this project on the Indiana SHPO staff is Rachel Sharkey and the structures reviewer is Danielle Kauffmann. However, if you have questions about our comments or about the review process, please contact initially the INDOT Cultural Resource Office staff members assigned to this project.

In any future correspondence regarding the CR 403 (Old SR 403) and Stacy Road intersection improvement project in Charlestown, Clark County, (Des. No. 1802805), please continue to refer to DHPA No. 25632.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:DMK:dmk

emc: Erica Tait, FHWA
Anuradha Kumar, INDOT
Susan Branigin, INDOT
Shaun Miller, INDOT
Anthony Ross, INDOT
Candace Hudziak, Metric Environmental, LLC
Diane Hunter, Miami Tribe of Oklahoma
Rachel Sharkey, DNR-DHPA
Danielle Kauffmann, DNR-DHPA

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COUNTY OF CLARK -SS**

Theresa Wheatbrook on oath says that she is
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State of Indiana, and further says that the annexed
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#(1) time(s) to-wit: In issue of said NEWS AND TRIBUNE

Dated: 02-10 2021

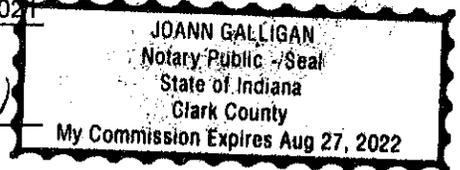
(X) Theresa Wheatbrook

**STATE OF INDIANA
COUNTY OF CLARK**

Subscribed and sworn to before me this

11TH day of February 2021

(X) Joann Galligan
Joann Galligan



Notary Public, Clark County, Indiana
(My Commission Expires August 27, 2022
Commission Number 655965

ID # 04-3314494

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**Public Notice
Des. No. 1802805**

Clark County is planning to proceed with an Intersection Improvement project funded in part by the Federal Highway Administration (FHWA). The project is located at the Intersection of CR 403 and Stacy Road in the City of Charlestown, Charlestown Township, Clark County, Indiana.

The preferred alternative would upgrade the existing intersection to a single-lane roundabout. The proposed roundabout is expected to have a diameter of 134 feet. The approaches along CR 403 (Old SR 403) and Stacy Road will include an 18-foot circulatory lane. A standard 8-foot truck apron will be provided. Located in the southwest quadrant of the intersection, the Pleasant Grove Methodist Church parking lot ingress/egress will be adjusted to ensure safe movement through the roundabout while maintaining access to the parking lot. New curb turnouts will be constructed, as needed, along the proposed curb lines with revetment riprap connecting to the existing roadside ditches. An additional 1.2 acres of permanent and 0.07 acre of temporary right-of-way will be required to construct the project.

The proposed action does not impact properties listed in or eligible for the National Register of Historic Places. INDOT, on behalf of the FHWA, has issued a "No Historic Properties Affected" finding for the project due to the fact that no historic properties are present within the Area of Potential Effects (APE). In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). The documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. Persons with limited Internet access may request project information to be mailed to them. Please contact Sam Snell, phone -- 317-912-3499, email -- sams@metricenv.com.

This documentation serves as the basis for the "No Historic Properties Affected" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Sam Snell of Metric Environmental, LLC (317-912-3499, sams@metricenv.com) no later than March 12, 2021.

In accordance with the "Americans with Disabilities Act", if you have a disability for which Clark County needs to provide accessibility to the document(s) such as interpreters or readers, please contact Brian Dixon, Highway Engineer, Clark County Government Building, 501 East Court Avenue, Room 404, Jeffersonville, IN 47130, phone -- 812-285-6286, email -- bdixon@co.clark.in.us. hspaxip

County Road 403 and Stacy Road Intersection Improvements
Des. No.: 1802805
Section 106 Finding of "No Historic Properties Affected"
Public Comments Received

1. TO WHOM IT MAY CONCERN,

Please LISTEN to the people of this state, and city and all who travel the roads, WE DO NOT WANT, LIKE OR NEED THESE STUPID ROUNDABOUTS !!! Whoever sold you this idea that a roundabout was a GREAT idea was WRONG !!! Please DO NOT put in another one ANYWHERE !!!

Respectfully,
Todd Standifer

Appendix E

Red Flag and Hazardous Materials



ENGINEERING
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 Jeffrey E. Lazzell, PE
 John SanGiorgio, PE

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 8440 Allison Pointe Blvd., Suite 200, Indianapolis, IN 46250

Date: March 13, 2020
 To: Site Assessment & Management
 Environmental Policy Office - Environmental Services Division
 Indiana Department of Transportation
 100 N. Senate Avenue, Room N642
 Indianapolis, IN 46204
 From: Aaron M. Toombs
 United Consulting
 8440 Allisonville Pointe Boulevard, Suite 200
 Indianapolis, Indiana 46250
 aaron.toombs@ucindy.com
 Re: RED FLAG INVESTIGATION
 Des. No.: 1802805
 County Road (CR) 403 at Stacy Road
 Intersection Improvement Project – Roundabout Construction
 Clark County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: Officials with Clark County intend to proceed with an intersection improvement project (Des. No.: 1802805). The proposed project includes constructing a roundabout at the intersection of CR 403 and Stacy Road. The roundabout will utilize a newly designed pavement section and is expected to have an Inscribed Circle Diameter (ICD) of 134.0 feet. The approaches along CR 403 and Stacy Road will be accommodated with an 18.0 foot circulatory lane for each leg of the roundabout. The construction of the roundabout is intended to provide drivers with a safer intersection by reducing vehicular speeds and reducing accident frequency and severity.

New curb turnouts will be constructed, as needed, along the proposed curb lines, with revetment riprap connecting to the existing roadside ditches. This allows the existing open ditch drainage to function as originally designed with minimal disturbance. Storm sewer design has not been included in the project scope; however, if recommended by the geotechnical report, new underdrains will be installed along the new curb lines. Additionally, including a pervious center island and pervious splitter islands are expected to reduce the overall runoff from the intersection. As a result, detention is not included in the project scope.

Existing utilities will be impacted by this project. Utilities expected to be impacted include AT&T distribution facilities, Clark County REMC power lines, Indiana American Water hydrant and waterline, and Vectren gas facilities. The impact to City of Chesterton facilities and Jeffersonville Wastewater and Sewer have not been fully determined. New, permanent lighting will be installed to assist navigation in addition to retroreflective pavement markings to aid during nighttime conditions.

Bridge and/or Culvert Project: Yes No Structure # N/A
 If this is a bridge project, is the bridge Historical? Yes No , Select Non-Select

Proposed right of way: Temporary # Acres <0.5 Permanent # Acres <0.5 Not Applicable

Type of excavation: Generally common dirt excavation to a depth less than 2 feet to remove existing pavement and prepare subgrade in widening areas. Bedrock excavation is possible but not expected.

Maintenance of traffic: Traffic will be maintained via a detour and will include closure of the intersection.

Work in waterway: Yes No Below ordinary high water mark: Yes No

State Project: LPA:

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	1	Recreational Facilities	N/A
Airports ¹	1	Pipelines	1
Cemeteries	2	Railroads	N/A
Hospitals	N/A	Trails	N/A
Schools	N/A	Managed Lands	N/A

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities: One (1) religious facility is located within the 0.5 mile search radius. The facility, Pleasant Grove Methodist Church, is located on the western corner of the intersection of CR 403 and Stacy Road. Coordination with Pleasant Grove Methodist Church will occur.

Airports: Although not mapped within the 0.5 mile search radius, one (1) public airport, Sellersburg Clark County, is located within 3.8 miles (20,000 feet) of the project area. The airport is approximately 3.8 miles southwest. Coordination with INDOT Aviation will occur.

Cemeteries: Two (2) cemeteries are located within the 0.5 mile search radius. The cemetery, mapped as two icons, called Pleasant Cemetery, is located 0.39 mile southwest of the project area. No impact is expected.

Pipeline: One (1) pipeline segment is located within the 0.5 mile search radius. The Indiana Gas Co. Inc. is located approximately 0.10 mile southeast of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	6
Canal Structures – Historic	N/A	Lakes	3
NPS NRI Listed	N/A	Floodplain - DFIRM	1
NWI-Lines	N/A	Cave Entrance Density	2
IDEM 303d Listed Streams and Lakes (Impaired)	4	Sinkhole Areas	3
Rivers and Streams	6	Sinking-Stream Basins	1

Explanation:

IDEM 303d Listed Streams and Lakes (Impaired): Four (4) IDEM 303d listed stream segments are located within the 0.5 mile search radius. The nearest IDEM 303d listed stream segment, associated with Silver Creek, is located 0.13 mile southwest of the project area. No impact is expected.

Rivers and Streams: Six (6) river and stream segments are located within the 0.5 mile search radius. The nearest river segment, associated with Silver Creek, is located 0.13 mile southwest of the project area. No impact is expected.

NWI – Wetlands: Six (6) NWI – wetlands are located within the 0.5 mile search radius. The nearest NWI – wetland, a freshwater pond, is located 0.14 mile northeast of the project area. No impact is expected.

Lakes: Three (3) lakes are located within the 0.5 mile search radius. The nearest lake is located approximately 0.14 mile northeast of the project area. No impact is expected.

Floodplain – DFIRM: One (1) floodplain polygon is located within the 0.5 mile search radius. The nearest floodplain is located approximately 0.29 mile southwest of the project area. No impact is expected.

Cave Entrance Density – Two (2) cave entrance density areas are located within the 0.5 mile search radius. The nearest cave entrance density area is located 0.35 mile south of the project area. No impact is expected.

Sinkhole Areas – Three (3) sinkhole areas are located within the 0.5 mile search radius. The nearest sinkhole area is located 0.03 mile northeast of the project area. Coordination with INDOT Ecology and Waterway Permitting will occur.

Sinking-Stream Basin – One (1) sinking-stream basin is located within the 0.5 mile search radius. The nearest sinking-stream basin is located 0.31 mile east of the project area. No impact is expected.

URBANIZED AREA BOUNDARY SUMMARY

Explanation: This project lies within the Clark County UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to 501 East Court Avenue, Jeffersonville, Indiana 47130.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	1	Mineral Resources	N/A
Mines – Surface	1	Mines – Underground	N/A

Explanation:

Petroleum Wells: One (1) petroleum well site is located within the 0.5 mile search radius. The Indiana Gas & Water Company well site is located 0.29 mile southeast of the project area. No impact is expected.

Mines – Surface: One (1) surface mine is located within the 0.5 mile search radius. The surface mine, Aggrock Quarry, is located approximately 0.45 mile from the project area. Due to the proposed MOT, which includes a road closure with a detour, coordination with Aggrock Quarry will occur.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	1	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	1
Solid Waste Landfill	1	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A

Explanation:

Underground Storage Tank (UST) Sites: One (1) UST site is located within the 0.5 mile search radius. The nearest UST site, associated with the Clark County Highway Department, 6103 SR 403, Charlestown, IN 47111 (FID: 14624), is located approximately 0.10 mile southwest of the project area. IDEM conducted an Underground Storage Tank Inspection on October 18, 2016, and the facility was found to be in compliance with equipment, operating, and maintenance requirements set forth in Indiana's UST Rule 329 IAC 9. No impact is expected.

Solid Waste Landfill: One (1) landfill boundary is located within the 0.5 mile search radius. The former Essroc Materials Inc. landfill (Al. ID: 7158), located at 301 US Highway 31, Sellersburg, IN 47172, approximately 0.49 mile southwest of the project area. According to the IDEM VFC, this site operated as a quarry and manufacturing area in the early 1900s. In 1994, four USTs were permanently closed at the site; 3 were removed, and one diesel UST was closed in place due to proximity to a building. Closure sampling results at the diesel UST indicated soil and groundwater impacts due to a product release. Site Characterization Approval was granted by IDEM January 19, 2010, and a Corrective Action Plan was developed by August Mack on May 21, 2010, which included further monitoring and product removal activities. An Environmental Restrictive Covenant was placed on the property in October of 2015. While contaminant concentrations have been vastly reduced at the site, groundwater flow is directed southwest from the site, and away from the project area. Therefore, no impact is expected.

Institutional Control Sites: One (1) Institutional Control site is located within the 0.5 mile search radius. Please see the above section 'Solid Waste Landfill' for further details regarding the site former Essroc Materials Inc. (Al. ID: 7158) site. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Clark County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of ETR species within the 0.5 mile search radius.

Due to the nature of project activities, this project will fall under the guidelines set forth under USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Religious Facilities: One (1) religious facility is located adjacent to the project area. Coordination with Pleasant Grove Methodist Church will occur.

Airports: Although not mapped within the 0.5 mile search radius, Sellersburg Clark County, is located within 3.8 miles (20,000 feet) of the project area. Coordination with INDOT Aviation will occur.

WATER RESOURCES:

One (1) sinkhole polygon is located approximately 0.03 mile northeast of the project area. Coordination with INDOT Ecology and Waterway Permitting will occur.

URBANIZED AREA BOUNDARY: This project lies within the Clark County UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to 501 East Court Avenue, Jeffersonville, Indiana 47130.

MINING/MINERAL EXPLORATION:

Mines – Surface: One (1) surface mine is located within the 0.5 mile search radius. Due to the proposed MOT, which includes a road closure with a detour, coordination with Aggrock Quarry will occur.

HAZARDOUS MATERIAL CONCERNS: N/A

ECOLOGICAL INFORMATION: The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to "Using the USFWS's IPaC System for Listed bat Consultation for INDOT Projects".

INDOT Environmental Services concurrence:
Prepared by:

Nicole Fohey-
Breting

Digitally signed by Nicole Fohey-
Breting
Date: 2020.03.15 14:43:45 -04'00'



Aaron M. Toombs
Environmental Specialist
United Consulting **DS**



Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached.

SITE LOCATION: YES

INFRASTRUCTURE: YES

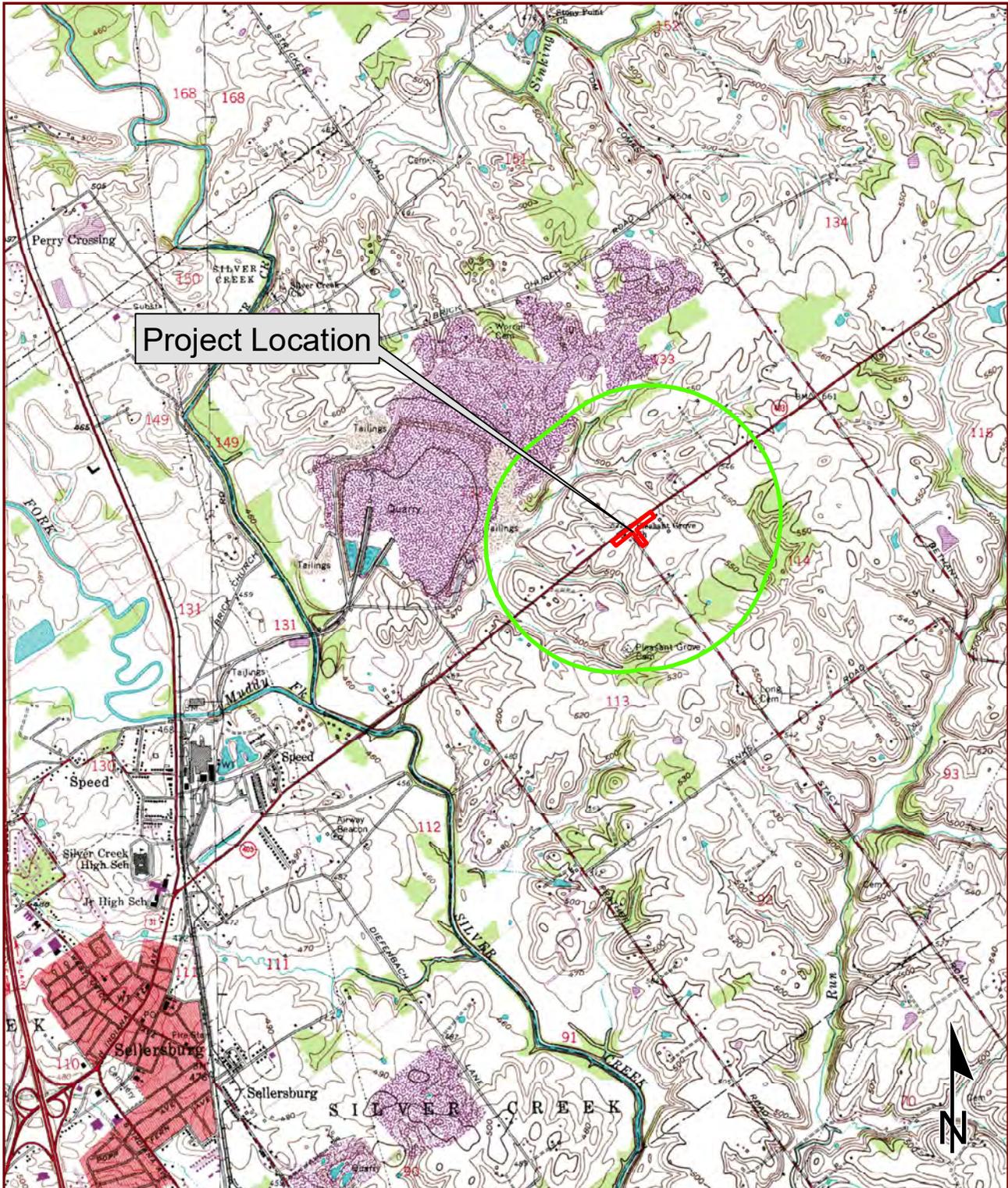
WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: YES

HAZARDOUS MATERIAL CONCERNS: YES

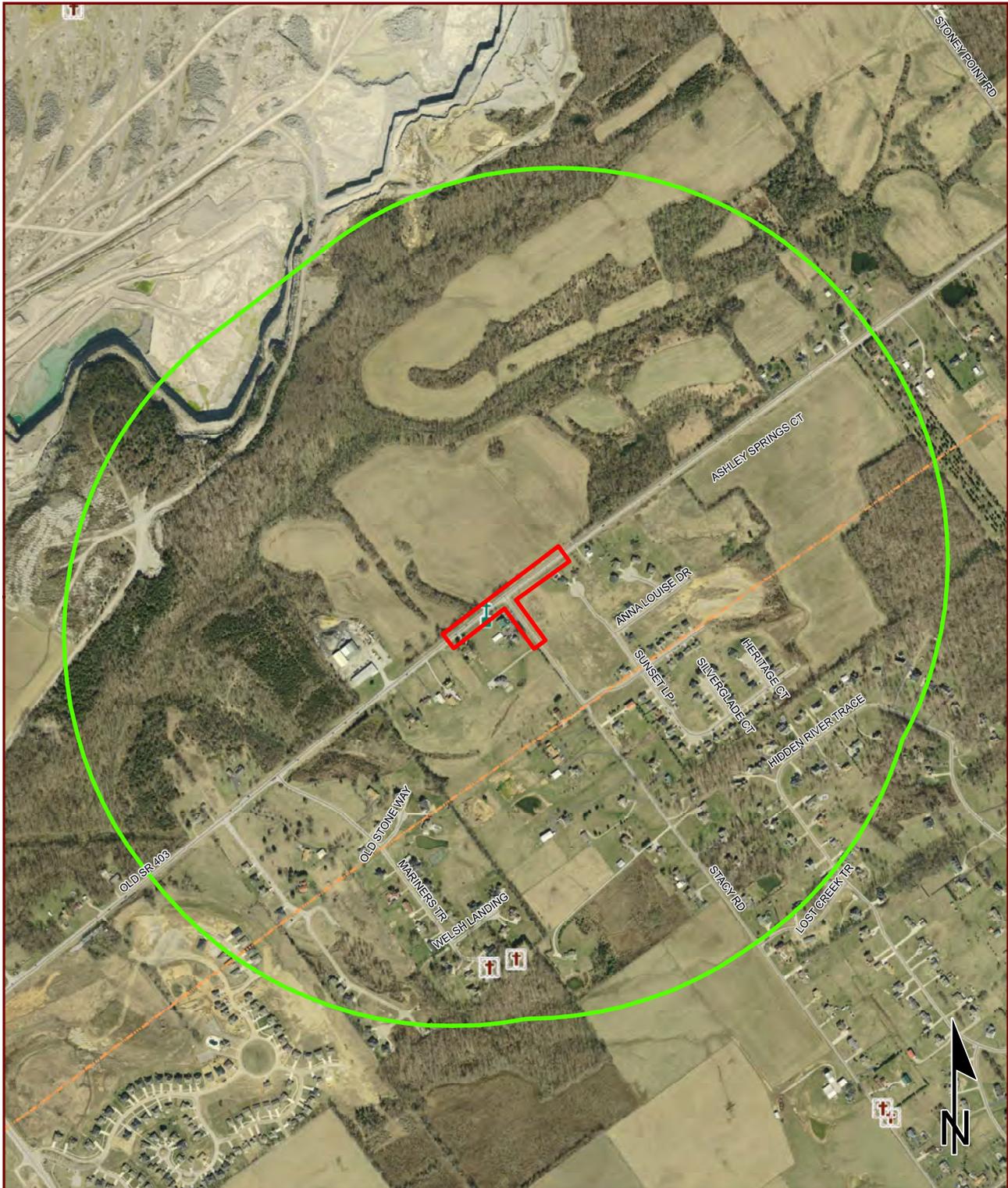
Red Flag Investigation - Site Location
 County Road 403 at Stacy Road
 Des. No.: 1802805 - Intersection Improvement Project
 Clark County, Indiana



Sources: 0.5 0.25 0 0.5 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**CHARLESTOWN QUADRANGLE
 INDIANA
 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

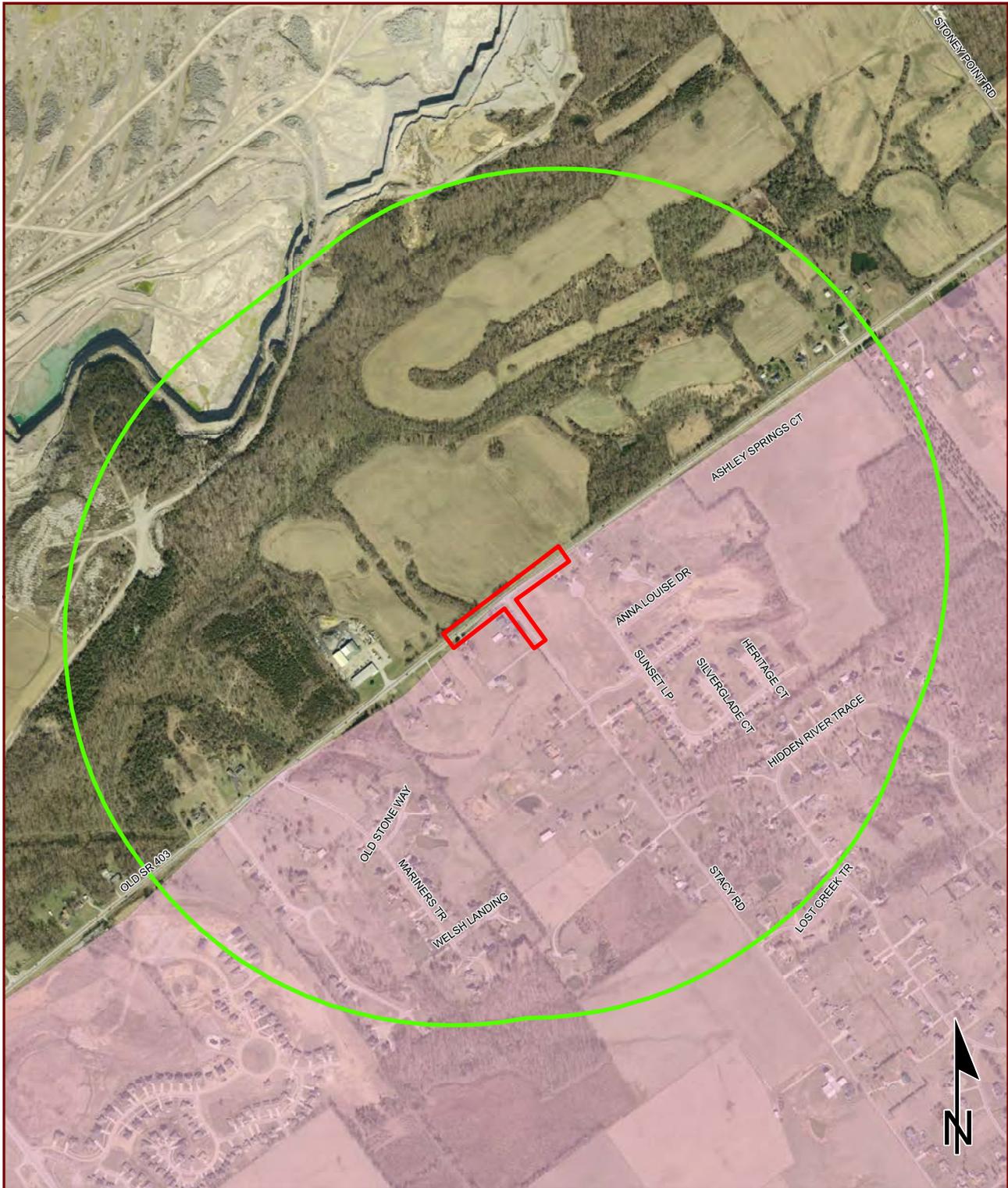
Red Flag Investigation - Infrastructure
 County Road 403 at Stacy Road
 Des. No.: 1802805 - Intersection Improvement Project
 Clark County, Indiana



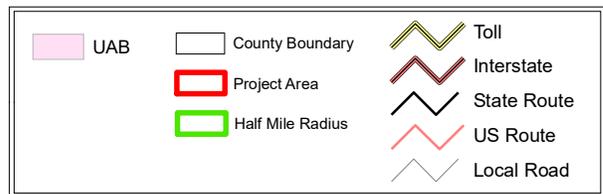
Sources: 0.15 0.075 0 0.15 Miles
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
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	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

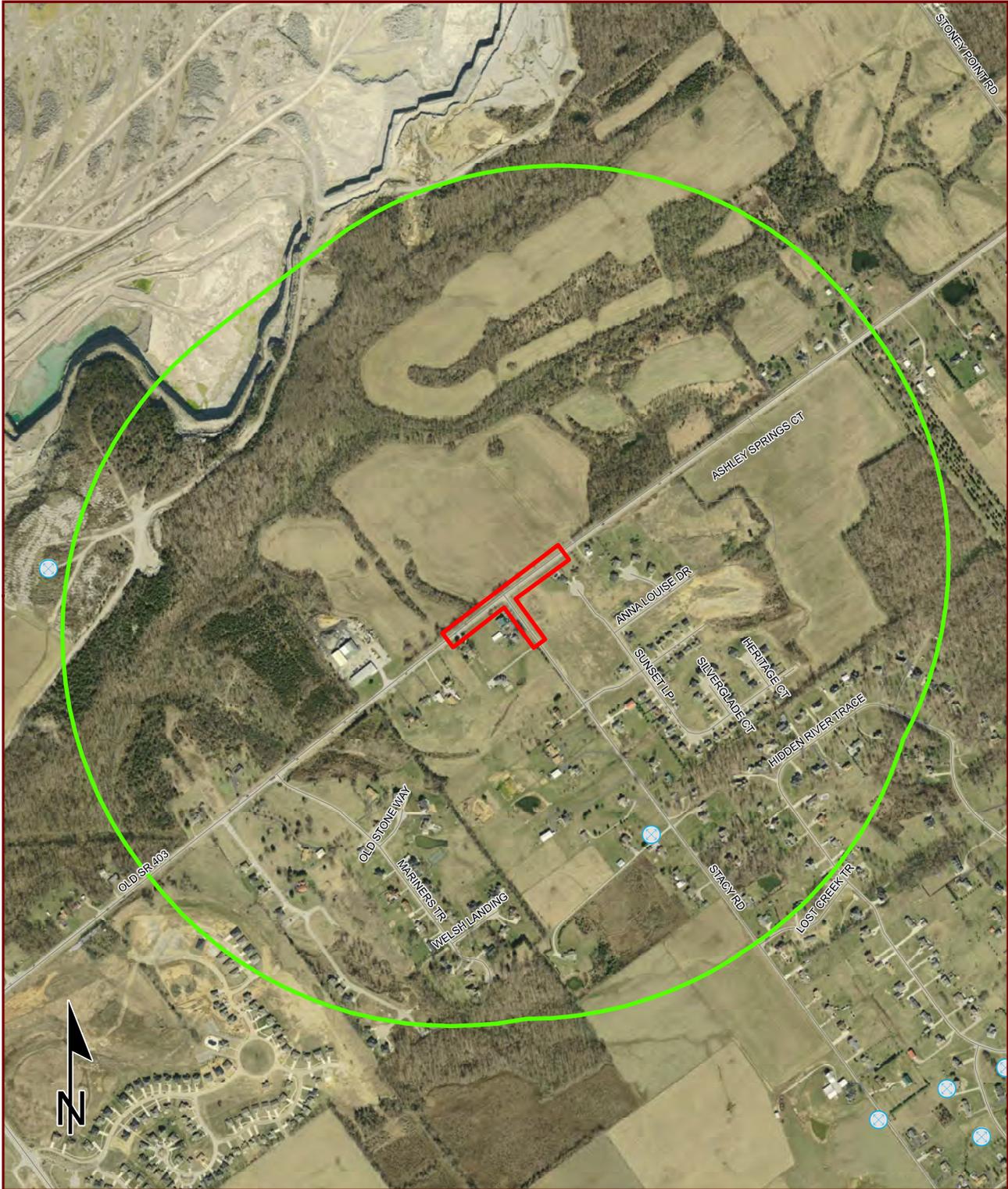
Red Flag Investigation - Urbanized Area Boundary
 County Road 403 at Stacy Road
 Des. No.: 1802805 - Intersection Improvement Project
 Clark County, Indiana



Sources: 0.15 0.075 0 0.15 Miles
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
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 Des. No.: 1802805



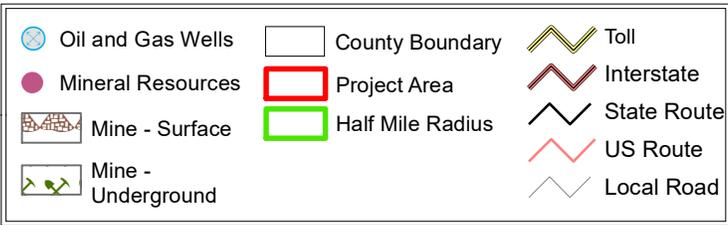
Red Flag Investigation - Mining and Mineral Resources
 County Road 403 at Stacy Road
 Des. No.: 1802805 - Intersection Improvement Project
 Clark County, Indiana



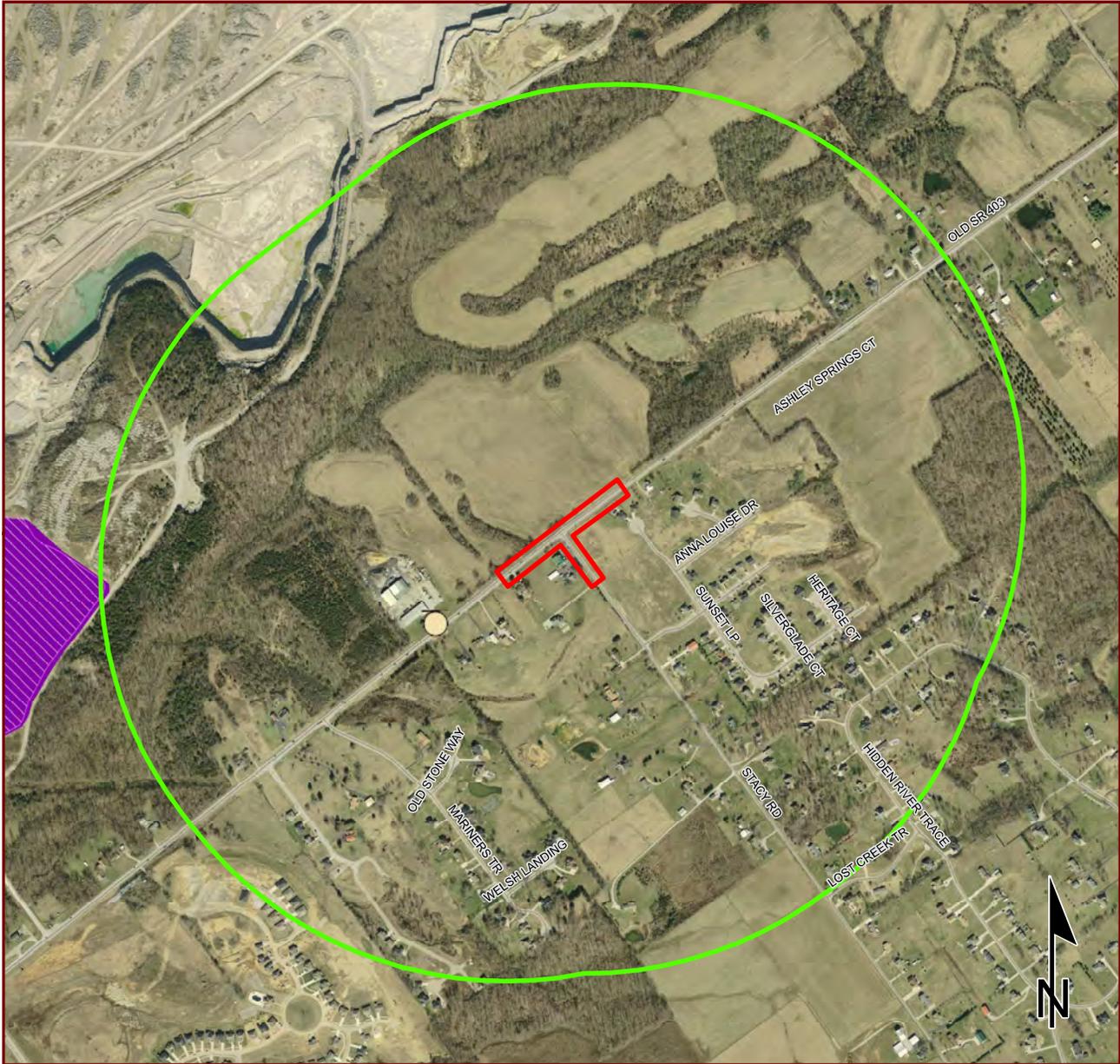
0.15 0.075 0 0.15 Miles

Sources:
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 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
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Red Flag Investigation - Hazardous Materials Concerns
 County Road 403 at Stacy Road
 Des. No.: 1802805 - Intersection Improvement Project
 Clark County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation Notice_of_Contamination		Septage Waste Site		Project Area
	Construction/Demolition Site		Solid Waste Landfill		Half Mile Radius
	Infectious/Medical Waste Site		State Cleanup Site		Toll
	Leaking Underground Storage Tank		Superfund		Interstate
	Manufactured Gas Plant		Tire Waste Site		State Route
	NPDES Facilities		Underground Storage Tank		US Route
	NPDES Pipe Locations		Voluntary Remediation Program		Local Road
	Open Dump Waste Site		Waste Transfer Station		

0.15 0.075 0 0.15
 Miles

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 Des. No.: 1802805

Red Flag and Hazardous Materials

Sources:
Non Orthophotography
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Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
Platyhelminthes (Flatworms)					
Sphalloplana weingartneri	Weingartner's Cave Flatworm		WL	G4	S3
Diplopoda					
Pseudotremia nefanda	Clark Cave Millepede		SE	G3G4	S2
Dipluran					
Campodea plusiochaeta	A Dipluran		SE	GNR	S1
Crustacean: Malacostraca					
Caecidotea jordani	Jordan's groundwater isopod		SE	G2G3	S1
Crangonyx ohioensis	An Amphipod			G1G2	S1
Crangonyx packardi	Packard's Cave Amphipod		WL	G4	S3
Gammarus bousfieldi	Bousfield's spring amphipod		SE	G1	S1
Stygobromus mackini	Mackin's cave amphipod		SE	G5	S1
Synurella dentata	Dentate amphipod		WL	GNR	S4
Crustacean: Copepoda					
Diacyclops jeanneli	Jeannel's Cave Copepod		ST	G3G4	S2
Mollusk: Bivalvia (Mussels)					
Fusconaia subrotunda	Longsolid	C	SX	G3	SX
Lampsilis fasciola	Wavyrayed Lampmussel		SSC	G5	S3
Potamilus capax	Fat Pocketbook	LE	SE	G2	S1
Villosa lienosa	Little Spectaclecase		SSC	G5	S3
Mollusk: Gastropoda					
Fontigens cryptica	Hidden Springs Snail		SE	G1	S1
Ellipluran: Collembola					
Pseudosinella fonsa	Fountain Cave Springtail		ST	G3G4	S2
Sinella alata	Springtail		WL	G5	S4
Sinella cavernarum	A Springtail		WL	G5	S3
Insect: Coleoptera (Beetles)					
Aleochara lucifuga	Rove beetle		WL	GNR	S4
Atheta annexa	Rove beetle		WL	G4	S4
Batrisodes krekeri	Krekeler's cave ant beetle		SE	G1	S1
Dryobius sexnotatus	Six-banded Longhorn Beetle		ST	GNR	S2
Pseudanophthalmus barri	Cave Beetle		SE	G1G2	S1
Insect: Lepidoptera (Butterflies & Moths)					
Celastrina nigra	Dusky Azure		SE	G4	S1
Pieris virginiensis	West Virginia White		SR	G3?	S3
Arachnida					
Dolomedes scriptus	Lined Nursery Web Spider			G5	S1?
Fish					
Acipenser fulvescens	Lake Sturgeon		SE	G3G4	S1

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
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Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
Etheostoma variatum	Variegate Darter		SE	G5	S1
Amphibian					
Acris blanchardi	Blanchard's Cricket Frog		SSC	G5	S4
Cryptobranchus alleganiensis alleganiensis	Eastern Hellbender	C	SE	G3G4T3T4	S1
Reptile					
Clonophis kirtlandii	Kirtland's Snake		SE	G2	S2
Crotalus horridus	Timber Rattlesnake		SE	G4	S2
Opheodrys aestivus	Rough Green Snake		SSC	G5	S3
Tantilla coronata	Southeastern Crowned Snake		SE	G5	S1
Terrapene carolina carolina	Eastern Box Turtle		SSC	G5T5	S3
Bird					
Aimophila aestivalis	Bachman's Sparrow			G3	SXB
Ammodramus henslowii	Henslow's Sparrow		SE	G4	S3B
Haliaeetus leucocephalus	Bald Eagle		SSC	G5	S2
Helmitheros vermivorus	Worm-eating Warbler		SSC	G5	S3B
Lanius ludovicianus	Loggerhead Shrike		SE	G4	S3B
Nycticorax nycticorax	Black-crowned Night-heron		SE	G5	S1B
Pandion haliaetus	Osprey		SSC	G5	S1B
Setophaga cerulea	Cerulean Warbler		SE	G4	S3B
Tyto alba	Barn Owl		SE	G5	S2
Mammal					
Mustela nivalis	Least Weasel		SSC	G5	S2?
Myotis grisescens	Gray Bat	LE	SE	G4	S1
Myotis sodalis	Indiana Bat	LE	SE	G2	S1
Sorex fumeus	Smoky Shrew		SSC	G5	S2
Sorex hoyi	Pygmy Shrew		SSC	G5	S2
Taxidea taxus	American Badger		SSC	G5	S2
Vascular Plant					
Acalypha deamii	Mercury		WL	G4?	S3
Asclepias viridis	Green Milkweed		SE	G4G5	S1
Asplenium resiliens	Black-stem Spleenwort		ST	G5	S2
Asplenium ruta-muraria	Wallrue Spleenwort		SR	G5	S3
Azolla caroliniana	Carolina Mosquito-fern		SR	G5	S3
Calamagrostis porteri ssp. insperata	Reed Bent Grass		SE	G4T3	S1
Carex eburnea	Ebony Sedge		SR	G5	S3
Carex straminea	Straw Sedge		ST	G5	S2
Chaerophyllum shortii	Wild Chervil		ST	G5T3T4Q	S2
Cirsium carolinianum	Carolina Thistle		SR	G5	S3
Clinopodium arkansanum	Calamint		ST	G5	S2

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SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
Cornus amomum ssp. amomum	Silky Dogwood		SE	G5T5	S1
Cuscuta indecora	Pretty Dodder		SE	G5	S1
Dichanthelium bicknellii	A Panic-grass		SE	G4?Q	S1
Eleocharis bifida	Glades spikerush		SE	G3G4	S1
Eurybia schreberi	Schreber Aster		SE	G4	S1
Heliotropium tenellum	Slender Heliotrope		ST	G5	S2
Hexalectris spicata	Crested Coralroot		SR	G5	S3
Hottonia inflata	Featherfoil		ST	G4	S2
Hylotelephium telephioides	Allegheny Stonecrop		SR	G4	S3
Iresine rhizomatosa	Eastern Bloodleaf		ST	G5	S3
Isoetes engelmannii	Appalachian Quillwort		SE	G4	S1
Lathyrus venosus	Smooth Veiny Pea		SE	G5	S1
Leavenworthia uniflora	Michaux Leavenworthia		SE	G4	S1
Lechea racemulosa	Illinois Pinweed		SE	G5	S1
Linum sulcatum	Grooved Yellow Flax		SR	G5	S3
Ludwigia decurrens	Primrose Willow		WL	G5	S3
Magnolia acuminata	Cucumber Magnolia		SE	G5	S1
Matelea obliqua	Angle Pod		SR	G4?	S3
Melica nitens	Three-flower Melic Grass		SE	G5	S1
Melothria pendula	Creeping Cucumber		ST	G5?	S2
Ophioglossum engelmannii	Limestone Adder's-tongue		SR	G5	S3
Oxalis illinoensis	Illinois Woodsorrel		WL	G4Q	S3
Passiflora incarnata	Purple Passion-flower		WL	G5	S3
Penstemon deamii	Deam Beardtongue		ST	G1	S2
Phlox amplifolia	Large-leaved Phlox		SR	G3G5	S3
Pleopeltis polypodioides	Resurrection Fern		WL	G5	S3
Rhexia mariana var. mariana	Maryland Meadow Beauty		ST	G5T5	S1
Scutellaria parvula var. australis	Southern Skullcap		WL	G4T4?	S2
Solidago squarrosa	Stout-ragged Goldenrod		SE	G4G5	S1
Spiranthes magnicamporum	Great Plains Ladies'-tresses		SE	G3G4	S1
Stachys clingmanii	Clingman Hedge-nettle		WL	G2	S3
Strophostyles leiosperma	Slick-seed Wild-bean		WL	G5	S3
Sullivantia sullivantii	Sullivantia		ST	G4	S2
Thalictrum pubescens	Tall Meadowrue		SR	G5	S3
Tragia cordata	Heart-leaved Noseburn		WL	G4	S3
Trifolium reflexum var. glabrum	Buffalo Clover		SE	G5T2T4Q	S1
Trifolium stoloniferum	Running Buffalo Clover	LE	SE	G3	S1
Valerianella chenopodiifolia	Goose-foot Corn-salad		WL	G4	S3
Viburnum molle	Softleaf Arrow-wood		SR	G5	S3
Viola hirsutula	Southern Wood Violet		SE	G4	S1

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
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SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: Clark

Species Name	Common Name	FED	STATE	GRANK	SRANK
High Quality Natural Community					
Barrens - bedrock limestone	Limestone Glade		SG	G4	S2S3
Barrens - bedrock siltstone	Siltstone Glade		SG	G2	S2
Forest - upland dry Highland Rim	Highland Rim Dry Upland Forest		SG	GNR	S3
Forest - upland dry-mesic Bluegrass	Bluegrass Dry-mesic Upland Forest		SG	GNR	S1
Forest - upland dry-mesic Highland Rim	Highland Rim Dry-mesic Upland Forest		SG	GNR	S3
Forest - upland mesic Bluegrass	Bluegrass Mesic Upland Forest		SG	GNR	S3
Forest - upland mesic Highland Rim	Highland Rim Mesic Upland Forest		SG	GNR	S3
Other Significant Feature					
Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade	Water Fall and Cascade			GNR	SNR

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Appendix F

Public Involvement



ENGINEERING
 ENVIRONMENTAL
 INSPECTION
 LAND SURVEYING
 LAND ACQUISITION
 PLANNING
 WATER &
 WASTEWATER
 SINCE 1965

OFFICERS
 William E. Hall, PE
 Dave Richter, PE, PLS
 Steven W. Jones
 Christopher R. Pope, PE
 B. Keith Bryant, PE
 Michael Rowe, PE

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 Andrew T. Wolka, PE
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 Dann C. Barrett, PE
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 8440 Allison Pointe Blvd., Suite 200, Indianapolis, IN 46250

November 8, 2019

RE: CR403 and Stacy Road Intersection Improvement
 Design Survey & Environmental Site Investigation
 Clark County, Indiana

Dear Property Owner,

County records indicate that you own or occupy property near this potential intersection improvement project. We have been retained by the Clark County for this project and our employees will be doing a field survey and environmental site investigation of the project area in the upcoming weeks. It may be necessary for them to come onto your property to complete the work. They will show you their identification, if you are available.

At this stage, we generally do not know what effect, if any, the project may eventually have on your property. If it is determined later that your property is involved, you will be contacted with additional information.

This work will include mapping the location of such features as roads, utilities, buildings, trees, fences, drives, obtaining spot elevations, identifying wetlands, archaeological resources, historic sites or other environmental resources. This work is needed for the proper planning and design of this intersection improvement project.

Please be assured of our sincere desire to cause you as little inconvenience as possible during this survey. If any problems do occur, please contact the undersigned, or our Project Manager, Mr. Jeff Lazzell, at the phone number or address shown to the left.

Sincerely,

UNITED CONSULTING

Tim Coomes, PLS
 Manager, Survey Dept.

c: file (19-408), B. Dixon

Appendix G

Air Quality

PROJECT LISTINGS

Project Name	Secondary Identifier	Description	Project Purpose	Primary Contact Agency	
Bridge Terminal Joints*		"Contract to repair bridge terminal joints throughout the Seymour District. Bridge NBI Numbers: 008960, 021560, 021561, 027620, 027630, 034350, 034360, 034400, 034410, 034420, 034430, 034440, 034470, 034480, 034490, 034500, 034510, 034513, 034610, 034611, 034616, 034640, 034670, 034680, 034690, 034700, 034704, 034706, 034708, 034720, 034730, 034740, 034750, 049638, 049640, 049652, 049654, 049655, 049656, 049657, 049658, 070530, 070540, 070550, 070560, 070570, 070580"	To repair bridge terminal joints throughout the Seymour District at various locations.	INDOT	
Bud Prather Road Section 130		Bud Prather Road railroad crossing warning device upgrades.	Improve railroad crossing safety for the motoring public.	INDOT	
Clark County Bridge Inspection		Bridge inspection of all bridges under Clark County jurisdiction.	County-wide bridge inspection for structure safety, maintenance and replacement.	Clark Co.	
Coopers Lane Railroad Grade Crossing		Railroad crossing warning device upgrade to train activated flashing lights with gates at Coopers Lane and US 31 at LIRC RR near Cementville.	To reduce crossing crash risk and add constant warning time train detection circuitry.	INDOT	
Coopers Lane Section 130		Coopers Lane railroad crossing warning device upgrades.	Improve railroad crossing safety for the motoring public.	INDOT	
CR 403 and Stacy Road Intersection Improvements		Intersection improvement including construction of a roundabout to improve safety at CR 403 and Stacy Road. Roundabout construction would include HMA pavement, curb and gutter and storm sewer for drainage, and intersection lighting. The footprint for the proposed roundabout would require approximately <1.0 acre of additional right of way, as well as relocating an existing Vectren utility pole and regulated gas line that runs along CR 403.	The purpose of the project is to reduce accidents at the subject intersection, and reduce overall speed along CR-403 corridor. Reports from FHWA indicate that a 25% reduction for property damage and a 75% reduction in injury/fatal crashes can be achieved by installing a roundabout. Accidents were studied during a 3-year period from 2014-2016 and 27 total accidents were reported. Manner of collisions were primarily 55% rear end type collisions, 15% ran off the road; and 30% classified as other, including turning collisions. Using RoadHat software, a benefit/cost ratio of 1.42 was calculated. This indicates that the project is a worthy candidate to include in the HSIP call for projects.	Clark Co.	
Curve Sign and Visibility Markings in Clark and Floyd Counties*		Curve sign and marking visibility improvements in the Seymour District at multiple locations including Clark and Floyd counties.	Seymour District Systemic Safety project for FY 2021 (Curve Sign Visibility marking).	INDOT	
East Utica Street Section 130		East Utica Street railroad crossing warning device upgrades.	Improve railroad crossing safety for the motoring public.	INDOT	

KIPDA ID #	State ID #	Parent Project	Group ID	Phase	Year	Federal	Other	Total	Federal Funding Category	Estimated Completion
2666	1901448		2676	C	2020	\$3,694,292	\$923,573	\$4,617,865	NHPP	2020
						\$3,694,292	\$923,573	\$4,617,865		
2656	1900376		2680	PE	2020	\$18,000	\$2,000	\$20,000	Rail Safety	2020
				C	2020	\$342,000	\$38,000	\$380,000	Rail Safety	
						\$360,000	\$40,000	\$400,000		
2418	1500201		2680	PE	2020	\$118,371	\$29,593	\$147,964	Bridge	NA
				PE	2021	\$3,480	\$870	\$4,350	Bridge	
						\$121,851	\$30,463	\$152,314		
2550	1801287		2680	C	2020	\$311,850	\$34,650	\$346,500	Rail Safety	2020
				PE	2020	\$34,650	\$3,850	\$38,500	Rail Safety	
						\$346,500	\$38,500	\$385,000		
2657	1801287		2680	C	2020	\$342,000	\$38,000	\$380,000	Rail Safety	2020
						\$342,000	\$38,000	\$380,000		
2549	1802805		2680	D	2020	\$283,500	\$31,500	\$315,000	HSIP-MPO	2024
				ROW	2022	\$82,500	\$9,167	\$91,667	HSIP-MPO	
				U	2023	\$108,000	\$12,000	\$120,000	HSIP-MPO	
				C	2024	\$1,923,750	\$213,750	\$2,137,500	HSIP-MPO	
						\$2,397,750	\$266,417	\$2,664,167		
2551	1801312		2680	PE	2020	\$45,000	\$5,000	\$50,000	HSIP-ST	2021
				C	2021	\$280,824	\$31,203	\$312,027	HSIP-ST	
						\$325,824	\$36,203	\$362,027		
2658	1900375		2680	C	2020	\$360,000	\$40,000	\$400,000	Rail Safety	2020
						\$360,000	\$40,000	\$400,000		

Indiana Department of Transportation (INDOT)
 State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Indiana Department of Transportation	41442 / 1801026	Init.	SR 160	Small Structure Replacement	E of SR 60 at the Clark/Scott County Line	Seymour	0	STPBG		Bridge Consulting	PE	\$473,920.00	\$118,480.00	\$578,000.00			\$14,400.00	
										Bridge ROW	RW	\$24,800.00	\$6,200.00		\$31,000.00			
Indiana Department of Transportation	41477 / 1800372	Init.	SR 3	HMA Overlay Minor Structural	2.74 miles N of SR 62 to SR 356	Seymour	13.025	STPBG		Road Construction	CN	\$4,982,727.20	\$1,245,681.80				\$6,228,409.00	
										Road Consulting	PE	\$192,000.00	\$48,000.00	\$240,000.00				
Indiana Department of Transportation	41478 / 1800375	Init.	US 31	Intersect. Improv. W/ Added Turn Lanes	At intersection with Perry Crossing Rd/Bud Prather Rd	Seymour	.083	STPBG		Safety Construction	CN	\$929,375.20	\$232,343.80			\$300,000.00	\$861,719.00	
										Safety ROW	RW	\$40,000.00	\$10,000.00		\$50,000.00			
Indiana Department of Transportation	41529 / 1600744	Init.	I 65	Replace Superstructure	2.68 miles S of SR 160 over Blue Lick Creek NBL	Seymour	0	NHPP		Bridge Construction	CN	\$3,301,956.00	\$366,884.00			\$3,668,840.00		
Indiana Department of Transportation	41529 / 1700135	Init.	I 65	Added Travel Lanes	2.78 miles S of SR 160 to SR 56	Seymour	12.433	NHPP		Bridge Construction	CN	\$3,237,891.30	\$359,765.70				\$3,597,657.00	
										Bridge Consulting	PE	\$396,000.00	\$44,000.00	\$440,000.00				
										Bridge ROW	RW	\$45,000.00	\$5,000.00		\$50,000.00			
										Road Construction	CN	\$89,769,087.90	\$9,974,343.10				\$99,743,431.00	
										Road Consulting	PE	\$4,050,000.00	\$450,000.00	\$3,000,000.00			\$1,500,000.00	
Indiana Department of Transportation	41533 / 1801686	Init.	SR 62	Slide Correction	3 miles E of SR 3.	Seymour	.03	STPBG		Road Construction	CN	\$227,046.40	\$56,761.60				\$283,808.00	
Clark County	41802 / 1802805	Init.	IR 1047	Intersection Improvement, Roundabout	CR 403 and Stacy Road Intersection Improvement	Seymour	0	NHPP		Local Funds	PE	\$0.00	\$31,500.00	\$31,500.00				
										Local Funds	RW	\$0.00	\$91,500.00	\$91,500.00				
										Local Funds	CN	\$0.00	\$333,750.00		\$120,000.00	\$213,750.00		
										Louisville MPO	PE	\$283,500.00	\$0.00	\$283,500.00				
										Louisville MPO	CN	\$1,923,750.00	\$0.00			\$1,923,750.00		
Indiana Department of Transportation	42018 / 1600716	Init.	I 65	Small Structure Replacement with Bridge	0.15 mile S of SR 311	Seymour	0	NHPP		Bridge Construction	CN	\$2,464,236.00	\$273,804.00		\$2,738,040.00			

Appendix H

Air Quality

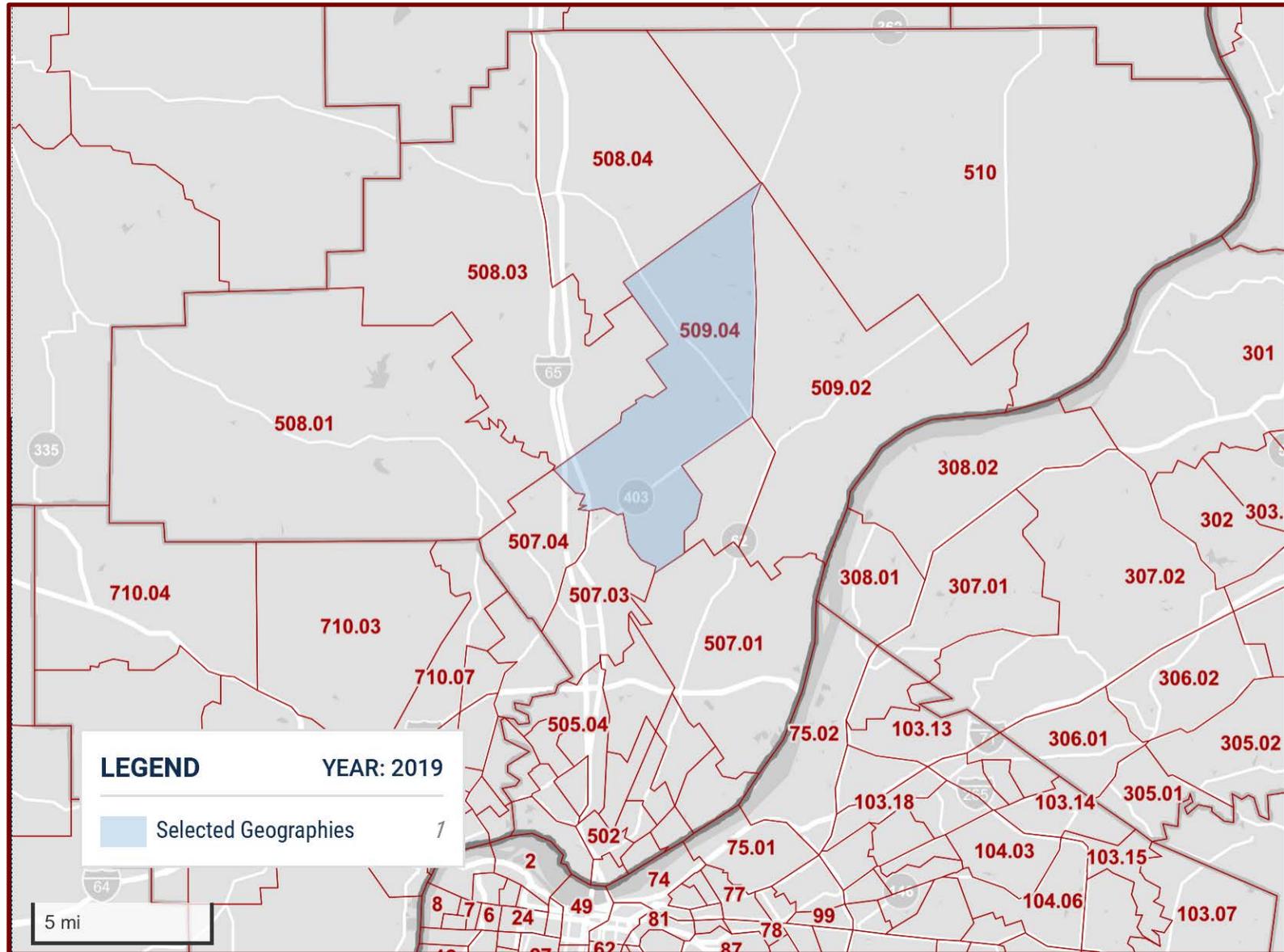
Des. No.: 1802805: County Road 403 and Stacy Road Intersection Improvement Project
Environmental Justice Data Analysis
Comparison of Clark County to Census Tract 509.04

	COC	AC1
	Clark County	Census Tract 509.04
LOW-INCOME POPULATION EJ ANALYSIS		
Population for whom poverty status is determined: Total	114,572	6658
Population for whom poverty status is determined: Income in 2018 below poverty level	11,544	396
Percent Low-Income	10.1%	5.9%
125 Percent of COC	12.6%	AC < 125% COC
Population of EJ Concern		No
MINORITY POPULATION EJ ANALYSIS		
Total population: Total	116,507	6,778
Total population: Not Hispanic or Latino	110,243	6,550
Total population: Not Hispanic or Latino; White alone	96,942	6,155
Total population: Not Hispanic or Latino; Black or African American alone	7,794	86
Total population: Not Hispanic or Latino; American Indian and Alaska Native alone	107	0
Total population: Not Hispanic or Latino; Asian alone	1,213	11
Total population: Not Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	13	0
Total population: Not Hispanic or Latino; Some other race alone	121	0
Total population: Not Hispanic or Latino; Two or more races	4053	298
Total population: Two races including Some other race	22	0
Total population: Two races excluding Some other race	4031	298
Total population: Hispanic or Latino	6,264	228
Total population: Hispanic or Latino; White alone	5,060	139
Total population: Hispanic or Latino; Black or African American alone	60	0
Total population: Hispanic or Latino; American Indian and Alaska Native alone	108	76
Total population: Hispanic or Latino; Asian alone	50	0
Total population: Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	0	0
Total population: Hispanic or Latino; Some other race alone	943	13
Total population: Hispanic or Latino; Two or more races	43	0
Total population: Two races including Some other race	10	0
Total population: Two races excluding Some other race	33	0
Number Non-white/minority	19565	623
Percent Non-white/minority	16.8%	9.2%
125 Percent of COC	21.0%	AC < 125% COC
Population of EJ Concern		No

Source: 2019 US Census Bureau

U.S. Census Boundary Map – Clark County, IN and Census Tract 509.04

<https://data.census.gov/cedsci/>



POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE				
TABLE ID:			B17001	
SURVEY/PROGRAM			American Community Survey	
PRODUCT:			ACS 5-Year Estimates Detailed Tables	
Clark County, Indiana			Census Tract 509.04, Clark County, Indiana	
Label	Estimate	Margin of Error	Estimate	Margin of Error
Total:	114572	382	6658	452
Income in the past 12 months below poverty level:	11544	1249	396	166
Male:	5146	668	188	92
Under 5 years	622	227	38	34
5 years	52	39	0	17
6 to 11 years	461	148	15	25
12 to 14 years	531	202	27	47
15 years	50	43	0	17
16 and 17 years	305	152	0	17
18 to 24 years	427	170	0	17
25 to 34 years	684	198	71	76
35 to 44 years	584	206	37	60
45 to 54 years	458	172	0	17
55 to 64 years	509	148	0	17
65 to 74 years	224	95	0	17
75 years and over	239	107	0	17
Female:	6398	769	208	87
Under 5 years	493	195	50	79
5 years	180	97	43	69
6 to 11 years	607	192	0	17
12 to 14 years	295	143	0	17
15 years	64	53	0	17
16 and 17 years	222	136	0	17
18 to 24 years	525	176	0	17
25 to 34 years	1064	239	62	53
35 to 44 years	797	209	0	17
45 to 54 years	596	169	0	17
55 to 64 years	687	168	29	35
65 to 74 years	509	128	24	39
75 years and over	359	137	0	17

Income in the past 12 months at or above poverty level:	103028	1280	6262	485
Male:	50472	736	3227	378
Under 5 years	3070	239	70	65
5 years	437	162	0	17
6 to 11 years	3516	368	276	164
12 to 14 years	2254	319	317	141
15 years	659	161	25	39
16 and 17 years	1226	183	135	101
18 to 24 years	3997	198	327	157
25 to 34 years	7250	234	339	129
35 to 44 years	6972	223	473	126
45 to 54 years	7151	167	507	119
55 to 64 years	6893	211	262	104
65 to 74 years	4651	111	304	119
75 years and over	2396	92	192	115
Female:	52556	793	3035	308
Under 5 years	2995	207	140	92
5 years	608	158	42	49
6 to 11 years	3562	354	244	128
12 to 14 years	1864	275	177	82
15 years	688	142	60	54
16 and 17 years	1144	179	60	48
18 to 24 years	4241	211	109	73
25 to 34 years	6779	262	348	150
35 to 44 years	7122	231	513	122
45 to 54 years	7031	209	428	115
55 to 64 years	7319	189	393	102
65 to 74 years	5654	159	390	107
75 years and over	3549	159	131	66

HISPANIC OR LATINO ORIGIN BY RACE				
TABLE ID:	B03002			
SURVEY/PROGRAM	American Community Survey			
PRODUCT:	ACS 5-Year Estimates Detailed Tables			
	Clark County, Indiana		Census Tract 509.04, Clark County, Indiana	
Label	Estimate	Margin of Error	Estimate	Margin of Error
Total:	116507	*****	6778	454
Not Hispanic or Latino:	110243	*****	6550	468
White alone	96942	126	6155	485
Black or African American alone	7794	534	86	68
American Indian and Alaska Native alone	107	77	0	17
Asian alone	1213	184	11	20
Native Hawaiian and Other Pacific Islander alone	13	22	0	17
Some other race alone	121	125	0	17
Two or more races:	4053	511	298	193
Two races including Some other race	22	24	0	17
Two races excluding Some other race, and three or more races	4031	511	298	193
Hispanic or Latino:	6264	*****	228	239
White alone	5060	498	139	199
Black or African American alone	60	73	0	17
American Indian and Alaska Native alone	108	132	76	122
Asian alone	50	55	0	17
Native Hawaiian and Other Pacific Islander alone	0	28	0	17
Some other race alone	943	484	13	23
Two or more races:	43	61	0	17
Two races including Some other race	10	22	0	17
Two races excluding Some other race, and three or more races	33	55	0	17

Appendix I

Environmental Justice

Land and Water Conservation Fund:
Grant Listings for Clark County, Indiana

**Grant Listings were retrieved from the INDOT Environmental Policy Webpage at (<https://www.in.gov/indot/2523.htm>), under Project Development Tools, titled, "The Land and Water Conservation Fund (LWCF) County Property List for Indiana".*

Clark County, Indiana

1800014	1800014	Clark	Henry Lansden Park
1800029	1800029	Clark	Northaven Park (Connie Sellmer)
1800029.1	1800029.1	Clark	Highland Park
1800041	1800041	Clark	Moser Park
1800053	1800053	Clark	Vissing Park
1800075	1800075	Clark	Henry Lansden Park
1800123	1800123	Clark	Deam Lake State Recreation Area
1800124	1800124	Clark	Lapping Park, Wooded View Golf Course
1800154	1800154	Clark	Deam Lake State Recreation Area
1800166	1800166	Clark	Deam Lake State Recreation Area
1800171	1800171AA	Clark	Deam Lake State Recreation Area
1800205	1800205	Clark	Lapping Park, Wooded View Golf Course
1800216	1800216	Clark	Vissing Park
1800248	1800248	Clark	Henry Lansden Park
1800305	1800305B	Clark	Deam Lake State Recreation Area
1800342	1800342	Clark	Lapping Park, Wooded View Golf Course
1800363	1800363E	Clark	Clark State Forest
1800363	1800363G	Clark	Deam Lake State Recreation Area
1800446	1800446	Clark	Clark State Forest
1800616	1800616	Clark	Borden Community Park